


THIS IS TO CERTIFY THAT THE SEWER FLOWS FOR THIS PROJECT WERE DESIGNED IN ACCORDANCE WITH THE REGIONAL TECHNICAL STANDARDS.

 DATE: 5-24-16

STEVE M. BLEVINS, P.E.

RESPONSIBLE LAND DISTURBER

THE FOLLOWING PERSON GARY FRANKS, PE (PRINT),

(SIGN), IS IDENTIFIED AS THE RESPONSIBLE LAND DISTURBER WHO WILL BE IN CHARGE OF AND RESPONSIBLE FOR CARRYING OUT THE LAND DISTURBING ACTIVITY. THIS PERSON MEETS THE APPLICABLE REQUIREMENTS OF VIRGINIA CODE SECTION 10.1-563 AND 10.1-566 BY VIRTUE OF THE FOLLOWING (CHECK THE CATEGORY THAT APPLIES):

RESPONSIBLE LAND DISTURBER CERTIFICATE

DCR CERTIFICATION FOR COMBINED ADMINISTRATOR, ADMINISTRATOR, PLAN REVIEWER, INSPECTOR, OR CONTRACTOR

☒ VA PROFESSIONAL ENGINEER, LAND SURVEYOR, LANDSCAPE ARCHITECT, OR ARCHITECT.

UPON AWARD OF THE CONTRACT AND BEFORE ANY LAND DISTURBING ACTIVITY CAN BEGIN, THE CONTRACTOR SHALL EXECUTE AND SUBMIT A RESPONSIBLE LAND DISTURBER NOTIFICATION FORM TO THE DEPARTMENT OF PLANNING, ENVIRONMENTAL SERVICES, RM. 508, CITY HALL BUILDING, 810 UNION STREET, NORFOLK, VA 23510 TEL: (757) 664-4368. AWARD OF THE CONTRACT WILL RELIEVE THE ABOVE SIGNER OF ALL RESPONSIBILITY.

PROJECT DESCRIPTION:

THE PROPOSED IMPROVEMENTS INCLUDE MILLING AND OVERLAYING THE ROADWAY AND RE-STRIP THE ROADWAY FROM A FOUR-LANE CORRIDOR TO A TWO-LANE WITH BIKE LANES. SIDEWALKS AND ADDITIONAL LANDSCAPE AREAS ARE INCLUDED AS PART OF THE STREETScape IMPROVEMENTS.

ADJACENT PROPERTY OWNER NOTIFICATION:

THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO ALL OWNERS AND RESIDENTS OF PROPERTY ADJACENT TO A DEVELOPMENT OR OFFSITE IMPROVEMENTS IN ACCORDANCE WITH THE FOLLOWING REQUIREMENTS:

- PROVIDE 30-DAY ADVANCED NOTICE TO PROPERTY OWNERS FOR CONSTRUCTION WITHIN THE RIGHT-OF-WAY WHEN PRIVATE IMPROVEMENTS ARE AFFECTED OR PAVEMENT WIDENING WILL OCCUR BETWEEN THE EXISTING PAVEMENT AND THE PROPERTY OWNER.
- PROVIDE 30-DAY ADVANCE NOTICE IF ANY CONSTRUCTION IS OCCURRING ON OFFSITE PRIVATE PROPERTY (INCLUDING EASEMENTS).
- WHEN PRIVATE IMPROVEMENTS ARE AFFECTED, THE NOTICE MUST INDICATE THE IMPACT ON THE PRIVATE IMPROVEMENTS. PRIVATE IMPROVEMENTS INCLUDE, BUT ARE NOT LIMITED TO, MAILBOXES, FENCES, ENTRANCES, LANDSCAPING, ETC.
- NO NOTICE IS REQUIRED FOR CONSTRUCTION THAT IS LIMITED TO THE DEVELOPMENT SITE AND RIGHT-OF-WAY WHERE PRIVATE IMPROVEMENTS ARE NOT AFFECTED.
- IN LIEU OF A REQUIRED 30-DAY NOTICE, THE CITY WILL ACCEPT A SIGNED ACKNOWLEDGEMENT THAT THE PROPERTY OWNER IS SATISFIED THAT THEY HAVE BEEN PROVIDED AN EXPLANATION OF THE PROJECT SCOPE OF WORK.

CITY OF NORFOLK - TOLERANCE NOTES:

- CONTRACTOR IS TO INSTALL ALL UTILITIES TO THE TOLERANCE REQUIRED BY THE CITY OF NORFOLK AND AS SHOWN ON THESE PLANS, SPECIFICALLY:
- ALL STORM DRAINAGE SHALL BE INSTALLED TO WITHIN 0.10' (ONE TENTH OF A FOOT) OF THE ELEVATION SHOWN ON THE PLAN, OR AS SPECIFIED IN WRITING BY THE CITY.
- MINIMUM STORM SEWER SLOPE SPECIFIED ON THE PLAN ARE ABSOLUTE MINIMUM; LESSER SLOPE WILL NOT BE ACCEPTED UNLESS IN WRITING BY THE CITY.
- CONTRACTOR TO BE RESPONSIBLE FOR ALL CORRECTION COST (I.E. PLAN, REVISION, PHYSICAL CORRECTION, ETC.) REQUIRED AS A RESULT OF INSTALLATION NOT MEETING THE REQUIRED TOLERANCE.

CITY OF NORFOLK - E&S NOTES:

- THE CONTRACTOR SHALL CONTACT THE CITY OF NORFOLK, BUREAU OF ENVIRONMENTAL SERVICES 664-4368 AT LEAST 48 HOURS PRIOR TO ANY LAND DISTURBING ACTIVITY SO THAT A LAND DISTURBING PERMIT CAN BE ISSUED AND A PRECONSTRUCTION CONFERENCE CAN BE SCHEDULED.

STORMWATER NOTES:

- AS-BUILTS OF ANY IMPROVEMENTS RELATED TO STORM WATER AND STREETScape ARE REQUIRED TO BE SUBMITTED TO THE DEPARTMENT OF PUBLIC WORKS – ENVIRONMENTAL STORMWATER FOR REVIEW AND APPROVAL PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY. FAILURE TO PROVIDE THE NECESSARY AS-BUILT INFORMATION WILL RESULT IN DELAYS. AS-BUILTS ARE REQUIRED TO BE PERFORMED AND SEALED BY A LICENSED LAND SURVEYOR SHOWING THE AS-BUILT LOCATION AND ELEVATIONS COMPARED TO THE PROPOSED.

CONTRACTOR TO CONTACT ENVIRONMENTAL STORM WATER MANAGEMENT @ 823-4000 (48 HOURS NOTICE) FOR PRE-CONSTRUCTION CONFERENCE AND INSPECTION REQUIREMENT WHEN BMP IS PROPOSED.



LAFAYETTE BOULEVARD

STREETSCAPE IMPROVEMENTS

Norfolk, VA

⚠ SITE PLAN #16-0025



PLANNING DEPARTMENT APPROVAL BLOCK

APPROVED BY THE CITY OF NORFOLK

DATE

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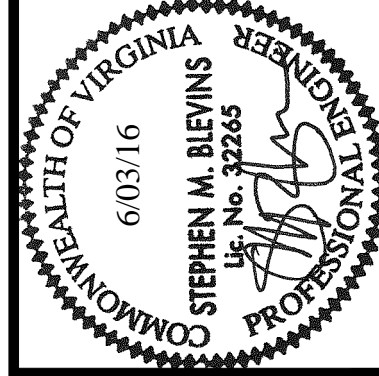
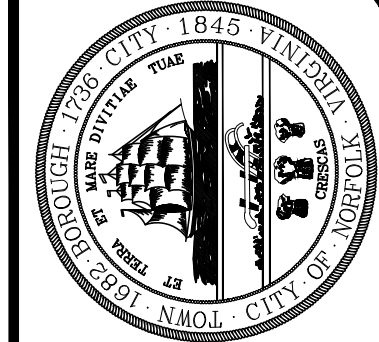
SITE INFORMATION

OWNER/DEVELOPER: CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
NORFOLK, VIRGINIA 23510
CONTACT: TAMMY HALSTEAD
PH # 757-664-4632

DESIGNER: BLAKeway CORPORATION
770 INDEPENDENCE CIRCLE, SUITE 100
VIRGINIA BEACH, VIRGINIA 23455
PH # 757-226-0081 FAX # 757-226-8765

LOCATION: THE PROPERTY AFFECTED IS LOCATED ALONG LAFAYETTE BOULEVARD FROM TIDEWATER DRIVE TO CHESAPEAKE BOULEVARD WITHIN THE PUBLIC RIGHT-OF-WAY

⚠ DISTURBED AREA: 66,079.85 SQ. FT. OR 1.51 ACRES ±



BLAKeway
surveying + engineering + environmental

770 INDEPENDENCE CIR - VA BEACH, VIRGINIA 23455
P/57.226.0081 - F/57.226.8765
www.blakewaycorp.com

CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

COVER SHEET

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CV-1.0

CITY OF NORFOLK - RIGHT-OF-WAY NOTES:

1.

ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL COMPLY WITH THE CITY OF NORFOLK RIGHT-OF-WAY EXCAVATION AND RESTORATION MANUAL, (ORDINANCE NO.40,778), DEPARTMENT OF PUBLIC WORKS RIGHT-OF-WAY DIVISION. DOWNLOAD AT [HTTP://WWW.NORFOLK.GOV/DOCUMENTCENTER/VIEW/779](http://www.norfolk.gov/documentcenter/view/779). IT IS IMPORTANT THAT CONTRACTORS FAMILIARIZE THEMSELVES WITH THE REQUIREMENTS OUTLINED IN THE MANUAL. FAILURE TO DO SO MAY IMPACT PROJECT COST. ALSO SEE "OTHER" IN THIS SECTION. EXCEPTIONS MAY BE PERMITTED. A REVIEW MEETING WITH RIGHT-OF-WAY IS REQUIRED. PLEASE CONTACT PUBLIC WORKS AT PWROW@NORFOLK.GOV TO SCHEDULE.
2.

ALL WORK WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH THE CITY OF NORFOLK DESIGN STANDARDS: DOWNLOAD AT [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=819](http://www.norfolk.gov/index.aspx?nid=819). EXCEPTIONS MAY BE PERMITTED. A REVIEW MEETING WITH RIGHT-OF-WAY IS REQUIRED. PLEASE CONTACT PUBLIC WORKS AT PWROW@NORFOLK.GOV TO SCHEDULE.
3.

FLOWABLE FILL IS NOT AN APPROVED MATERIAL FOR USE IN THE CITY OF NORFOLK FOR BACKFILL WITHOUT PRIOR APPROVAL. THE SELECT MATERIAL AND 21A STONE WILL BE IN ACCORDANCE WITH THE 2002 VDOT ROAD AND BRIDGE SPECIFICATIONS. FOR BACKFILL AROUND THE PIPE USE SELECT MATERIAL (SECTION 207) – TYPE 1 AND 20 CBR COMPACTION. FOR SUB BASE MATERIAL USE 6" MIN. VDOT 21A STONE IN ACCORDANCE TO SECTION 208.
4.

NEW DEVELOPMENTS, CONSTRUCTION AND ADDITIONS REQUIRE NEW SIDEWALK, CURB, GUTTER, AND DRIVEWAYS. EXCEPTIONS MAY BE PERMITTED.A REVIEW MEETING WITH RIGHT-OF-WAY IS REQUIRED. PLEASE CONTACT PUBLIC WORKS AT PWROW@NORFOLK.GOV TO SCHEDULE.
5.

IF REQUIRED BY RIGHT-OF-WAY, WHEN THE CONTRACTORS WORK REQUIRES THE SAW CUTTING AND/OR PARTIAL DEMOLITION AND/OR REMOVAL OF ANY SECTION OF AN EXISTING APRON OR SIDEWALK, THE CONTRACTOR WILL BE REQUIRED TO REMOVE EXISTING APRON OR SIDEWALK AND REPLACE IT WITH A NEW MONOLITHIC APRON OR SIDEWALK. SIDEWALK SHALL BE REMOVED TO THE NEAREST JOINT. ALL CONCRETE IS TO BE SAW CUT. NO PATCHING WILL BE ALLOWED. COMMERCIAL APRONS SHALL BE REPLACED WITH AN APPROVED COMMERCIAL APRON. RESIDENTIAL APRONS SHALL BE REPLACED WITH AN APPROVED RESIDENTIAL APRON (HS-207). SIDEWALKS SHALL BE REPLACED WITH AN APPROVED SIDEWALK DETAIL. CONTACT THE DIVISION OF RIGHT-OF-WAY PERMIT OFFICE FOR APPROVAL AT PWROW@NORFOLK.GOV (ORDINANCE NO: 40,778).
6.

UTILITY POLES MUST BE RELOCATED AT YOUR PROJECT’S COST. A COPY OF THE WORK ORDER FOR POLE RELOCATIONS MUST ACCOMPANY RIGHT-OF-WAY PERMITS.

PERMITS

1.

A PERMIT IS REQUIRED TO PERFORM EXCAVATION AND INSTALLATION WORK OF ANY KIND IN THE RIGHT-OF-WAY. APPLICATIONS: [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
2.

A PERMIT IS REQUIRED TO SLOW, CLOSE, REDIRECT, DETOUR, OR ALTER VEHICULAR AND PEDESTRIAN TRAFFIC FOR ANY DURATION. APPLICATIONS: [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
3.

A PERMIT IS REQUIRED FOR LANE OR SIDEWALK CLOSURES FOR WORK WASHING, GRADING, OR INSTALLING ANY ITEM ABOVE OR UNDERGROUND. APPLICATIONS: [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
4.

A PERMIT IS REQUIRED TO PLACE ANY OBJECT IN THE PUBLIC RIGHT-OF-WAY. APPLICATIONS: [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
5.

A PERMIT IS REQUIRED TO HAUL OVERSIZED, OVERWEIGHT, OR HEIGHT LOADS. APPLICATIONS: [HTTP://WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).

FEES

1.

CITY CONTRACTORS: EXEMPT FROM PERMIT FEES THROUGH JULY 1, 2014. EFFECTIVE JULY 1, 2014 CITY CONTRACTORS MUST PAY FOR STREET, LANE, AND SIDEWALK CLOSURES.
2.

CURRENT RIGHT-OF-WAY PERMIT FEES: [HTTP://WWW.NORFOLK.GOV/DOCUMENTCENTER/VIEW/789](http://www.norfolk.gov/documentcenter/view/789)

TRAFFIC CONTROL

1.

ALL TRAFFIC CONTROL SHALL COMPLY WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)", THE "VIRGINIA WORK AREA PROTECTION MANUAL", AND THE CITY OF NORFOLK RIGHT-OF-WAY EXCAVATION AND RESTORATION MANUAL. ADDITIONAL CHARGES APPLY IF OFF-DUTY OFFICERS ARE REQUIRED. A LIST OF PERMIT FEES IS LOCATED AT [WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
2.

FOR CONSTRUCTION IN OR ADJACENT TO ANY ARTERIAL AND /OR COLLECTOR STREETS REQUIRING TEMPORARY LANE OR STREET CLOSURE, THE CONTRACTOR MUST SCHEDULE A MEETING WITH THE CITY’S TRAFFIC MITIGATION TEAM. PLEASE CONTACT PUBLIC WORKS AT PWROW@NORFOLK.GOV TO SCHEDULE.
3.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT A DETAILED TRAFFIC CONTROL PLAN (MOT) WITH THE CONTRACTOR’S PERMIT APPLICATION TO WORK IN THE RIGHT-OF-WAY AT LEAST 14 DAYS IN ADVANCE OF THE START DATE. THE MOT SHALL BE A DRAWING OR AERIAL PHOTO WITH SITE SPECIFIC DETAILS. THESE SHALL INCLUDE BUT ARE NOT LIMITED TO: TRAFFIC SIGNS, DIRECTIONAL SIGNS, DESTINATION SIGNAGE, CHANNELIZING DEVICES, WATER-FILLED BARRIERS, PORTABLE MESSAGE BOARDS WITH MESSAGES TO BE DISPLAYED AND TIMES AND DURATIONS OF DISPLAYED MESSAGES. ADDITIONALLY THE MOT SHALL DISPLAY THE EXACT LOCATION OF ALL SIGNS AND/OR DEVICES INCLUDING SPACING FOR EACH SIGN AND/OR DEVICE, SPACING BEING APPROPRIATE FOR WORK ZONE LOCATION AND POSTED SPEED LIMITS. ALL MAINTENANCE OF TRAFFIC (MOT) PLANS SHALL CONFORM THE THE CURRENT ISSUE OF "THE VIRGINIA WORK AREA PROTECTION MANUAL". COPIES OR REPRODUCTIONS OF STANDARD VWAPM TTC PLANS MAY NOT BE SUFFICIENT. CONTRACTOR SHOULD CONSIDER THESE ITEMS WHEN BIDDING FOR CONSTRUCTION AS IT MAY AFFECT COST.
4.

NORFOLK POLICE REQUIRES THAT OFF-DUTY OFFICERS FOR TRAFFIC CONTROL IN ANY SIGNALIZED INTERSECTION AND AS DIRECTED BY PERMIT BE SCHEDULED THROUGH THE SUPERVISOR OF THE TRAFFIC UNIT AT 757-823-4464.

BONDS

1.

THE CONTRACTOR, OWNER, OR DEVELOPER WILL BE REQUIRED TO POST A CASH, CHECK, OR SURETY BOND OR LETTER OF CREDIT TO EXCAVATE IN THE RIGHT-OF-WAY. DOWNLOAD THE FORM: [HTTP://WWW.NORFOLK.GOV/DOCUMENTCENTER/VIEW/1406](http://www.norfolk.gov/documentcenter/view/1406).
2.

BOND AMOUNT SHOULD INCLUDE 100% TOTAL COST OF ROW INSTALLATION INCLUDING STREETLIGHTS, WATER, SEWER, STORM, BMP’S, ROADWAY, SIDEWALK, CURB, GUTTER.
3.

BOND WILL BE REDUCED UP TO 10% FROM THE DATE OF FINAL INSPECTION FOR A 2-YEAR WARRANTY TERM. A SEPARATE WARRANTY BOND IS ACCEPTABLE IN LIEU OF A BOND REDUCTION. A LETTER REQUESTING RELEASE OR REDUCTION MUST BE SENT TO THE RIGHT-OF-WAY PERMITS OFFICE.
4.

ALL CITY PROJECTS CAN NOT BE BID UNTIL UTILITIES ARE RELOCATED OR ARRANGEMENTS HAVE BEEN COORDINATED BY THE CITY’S PROJECT MANAGER, A/E. AND RIGHT-OF-WAY DIVISION. PLEASE PROVIDE A LIST AND DRAWINGS OF ALL UTILITIES NEEDING RELOCATION TO RIGHT-OF-WAY DIVISION AT OR BEFORE 30% SUBMITTAL.
5.

PROJECT DESIGNERS MUST INCLUDE A DETAILED TRAFFIC CONTROL PLAN FOR UTILITY WORK WITHIN THE RIGHT-OF-WAY IN THE PLANS. FOR CONSTRUCTION IN OR ADJACENT TO ANY ARTERIAL AND/OR COLLECTOR STREETS AS DEFINED IN THE VIRGINIA WORK AREA PROTECTION MANUAL REQUIRING TEMPORARY LANE OR STREET CLOSURE, THE CONTRACTOR MUST SCHEDULE A MEETING WITH THE CITY’S TRAFFIC MITIGATION TEAM. PLEASE CONTACT PUBLIC WORKS AT PWROW@NORFOLK.GOV TO SCHEDULE.

OTHER:

1.

DETAILED STAMPED DRAWINGS FOR SHORING AND SCAFFOLDING WITH BE REQUIRED AS PART OF THE RIGHT-OF-WAY PERMIT PROCESS.
2.

ANY EXCAVATION IN THE ROADWAY MUST BE PATCHED WITH ASPHALT. STONE IS NOT ACCEPTABLE.
3.

FULL CURB TO CURB RESURFACING WILL BE REQUIRED IF ROADWAY PORTION IS CUT WITHIN THE FIRST 5 YEARS AFTER RESURFACING. AN ADMINISTRATIVE FEE OF \$500 WILL BE ASSESSED IN ACCORDANCE WITH CITY CODE.
4.

WHEN A NEW APRON AND/OR SIDEWALK ARE INSTALLED, ALL NEW OR EXISTING UTILITY STRUCTURES SHALL BE INSTALLED AND/OR RELOCATED OUTSIDE THE AREA OF THE NEW APRON AND/OR SIDEWALK. A PERMIT AND INSPECTION ARE REQUIRED. PLEASE CONTACT THE RIGHT-OF-WAY PERMITS OFFICE AT 757-664-7306 OR AT [WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362)
5.

AS PART OF THE CONTRACTOR’S PERMIT APPLICATION TO WORK IN THE RIGHT-OF-WAY, THE CONTRACTOR MUST SUBMIT A MAP IDENTIFYING THE PROJECTS STORAGE AND LAY DOWN AREA. THE CITY DOES NOT ALLOW STORAGE, STAGING OR LAY DOWN AREAS WITHIN THE RIGHT-OF-WAY. CONTRACTORS ARE ENCOURAGED TO MAKE ARRANGEMENTS TO STORE MATERIALS AND LAY DOWN ON PRIVATE PROPERTY OR THEY MUST BE STORED WITHIN CONSTRUCTION SITE.
6.

ALL MISS UTILITY MARKINGS IN THE RIGHT-OF-WAY ARE REQUIRED TO BE ERADICATED AT THE COMPLETION OF THE PROJECT IN ACCORDANCE WITH THE CITY’S ROW EXCAVATION & RESTORATION MANUAL LOCATED AT [WWW.NORFOLK.GOV/INDEX.ASPX?NID=362](http://www.norfolk.gov/index.aspx?nid=362).
7.

DESIGNERS ARE RESPONSIBLE TO CHECK IN-FIELD GRADES AND TO CONSULT WITH THE PROPER CITY OF NORFOLK STAFF IF THERE ARE QUESTIONS OR CONCERNS. DESIGNERS MUST SUBMIT A NOTICE VIA EMAIL TO THE RIGHT-OF-WAY DIVISION AT PWROW@NORFOLK.GOV VERIFYING THAT GRADES HAVE BEEN CHECKED AND ARE CORRECT.

NOTIFICATIONS

1.

NOTIFICATIONS MUST BE APPROACHED FROM 3 LEVELS OR TIERS FOCUSING ON THOSE AFFECTED BY THE CONSTRUCTION. CONTRACTORS AND/OR PROJECT SPONSOR WILL BE REQUIRED TO NOTIFY RESIDENTS AND BUSINESSES IN THE IMMEDIATE AREA THAT WILL BE AFFECTED BY THE CONSTRUCTION. DOOR HANGERS MUST BE PLACED AT LEAST 48 HOURS TO CONSTRUCTION AND MUST INCLUDE A 24 HOUR CONTACT PHONE NUMBER FOR EMERGENCIES. UPDATES MUST BE PROVIDED TO RESIDENTS AND BUSINESSES ON A BI-WEEKLY OR MONTHLY BASIS DEPENDING ON THE FOLLOWING:
- A)

IMMEDIATE IMPACT – BUSINESS OR RESIDENTS WITHIN THE CONSTRUCTION ZONE WHO WILL BE AFFECTED BY ACCESS RESTRICTIONS, NOISE, DIRT AND DUST, USE OF PROPERTY DIRECTLY ADJACENT TO THEM, ETC.
- B)

AREA IMPACT – THOSE WHO TRAVEL THROUGH OR WILL BE DETOURED AROUND LOCAL OR COLLECTOR STREETS, AFFECTED BY CONSTRUCTION INCLUDING CIVIC LEAGUES, BUSINESS ASSOCIATIONS AND PROPERTY OWNERS.
- C)

CITYWIDE IMPACT – THOSE WHO ARE AFFECTED BY VEHICULAR OR PEDESTRIAN ACCESS ON ARTERIAL STREETS OR IN BUSINESS OR OTHER PUBLIC ACTIVITY AREAS (SCHOOLS, UNIVERSITIES, GOVERNMENT FACILITIES, ETC.)

CITY OF NORFOLK - GENERAL NOTES:

1.

ALL CONSTRUCTION METHODS AND MATERIALS IN THE PUBLIC RIGHT OF WAY SHALL CONFORM WITH THE CURRENT SPECIFICATIONS AND STANDARDS OF THE DEPARTMENTS OF PUBLIC WORKS, UTILITIES, CITY OF NORFOLK, VIRGINIA AND HAMPTON ROADS PLANNING DISTRICT COMMISSION LATEST 5th EDITION REGIONAL STANDARD SPECIFICATION AS AMENDED BY THE CITY OF NORFOLK DEPARTMENT OF UTILITIES STANDARD SPECIFICATION AND THE LATEST EDITION OF THE VIRGINIA DEPARTMENT OF HEALTH REGULATIONS.
2.

PRIOR TO CONSTRUCTION OR EXCAVATION IN THE PUBLIC RIGHT OF WAY, THE CONTRACTOR SHALL ASSUME THE RESPONSIBILITY OF LOCATING ANY UNDERGROUND UTILITIES (PUBLIC AND PRIVATE) THAT MAY EXIST AND CROSS THROUGH THE AREA OF CONSTRUCTION THAT ARE NOT SHOWN ON THESE PLANS. BEFORE YOU DIG, TO MISS THE UTILITIES, CALL "MISS UTILITY OF VIRGINIA" AT 811. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING AT THE EXPENSE ANY EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
3.

FOR WORK IN THE CITY OF NORFOLK’S RIGHT OF WAY, CONTACT 48 HOURS IN ADVANCE:
DIVISION OF RIGHT OF WAY (PERMITS): 664-7306 INSPECTIONS: 664-7306
DRAINAGE: 823-4000 LANDSCAPING: 823-4023
UTILITIES: 664-6700
4.

APPROVAL DOES NOT PRECLUDE THE NECESSITY OF THE EXCAVATOR, BY STATE LAW, TO CONTACT THE DIVISION OF TRANSPORTATION 48 HOURS BEFORE EXCAVATION AT 664-7306. (PERMIT NUMBER REQUIRED)
5.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING WITH MATCHING MATERIALS ANY PAVEMENT, DRIVEWAYS, WALKS, CURBS, ETC. THAT MAY BE CUT, OR THAT ARE DAMAGED DURING CONSTRUCTION.
6.

MANHOLE COVERS AND MISCELLANEOUS COVERS AND LIDS ARE TO BE RE-ESTABLISHED AT PROPOSED GRADES.
7.

PROJECT LIMITS ARE THE PROPERTY LINES, UNLESS OTHERWISE NOTED.
8.

TOPSOIL, SEED AND FERTILIZE ALL AREAS WITHIN THE PROJECT LIMITS NOT OCCUPIED BY STRUCTURES, PAVEMENT, SIDEWALKS, ETC. ALL LANDSCAPING SHALL BE PROVIDED AND INSTALLED BY THE OWNER. CONTRACTOR SHALL COORDINATE WITH THE OWNER FOR PLACING TOPSOIL AND SEEDING TO NOT CONFLICT WITH PLANTING AREAS.
9.

NO LANE CLOSURES AND/OR WORK IN THE RIGHT-OF-WAY, INCLUDING GRADING, SIDEWALK, CURB AND GUTTER, AND DRIVEWAY APRON INSTALLATION WILL BE ALLOWED WITHOUT A PERMIT FROM THE DIVISION OF RIGHT OF WAY (664-7306). ALL TRAFFIC CONTROL SHALL COMPLY WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)", THE "VIRGINIA WORK AREA PROTECTION MANUAL" AND THE CITY OF NORFOLK RIGHT OF WAY EXCAVATION AND RESTORATION MANUAL. ADDITIONAL CHARGES APPLY IF OFF-DUTY OFFICERS ARE REQUIRED. A LIST OF PERMIT FEES IS LOCATED AT [WWW.NORFOLK.GOV/PUBLICWORKS/ROW](http://www.norfolk.gov/publicworks/row).
10.

DRIVEWAY APRON IS TO BE NORFOLK STANDARD HS-207 OR VDOT STANDARD CG-13, 8" FOR COMMERCIAL. ALL CONCRETE TO CLASS A-3 AIR ENTRAINED (3,000 PSI). MAINTAIN EXISTING ELEVATION AT STREET/EDGE OF SIDEWALK. REMOVE SIDEWALK AND FORM TO UNIFORM THICKNESS. REMOVE CURB TO BASE. POUR TO BE INTEGRAL WITH APRON. ALL CONCRETE TO BE SAW CUT.
11.

THE PROPOSED GROUND FLOOR ELEVATION OF ALL BUILDINGS SHALL BE 2 FOOT ABOVE THE BASE FLOOD ELEVATION AS SHOWN ON THE HUD FLOOD ZONE HAZARD MAPS FOR THE CITY OF NORFOLK, VIRGINIA.
12.

ADEQUATE LIGHTING SHALL BE PROVIDED FOR EXTERIOR OF BUILDINGS AND/OR PARKING LOTS IN SUCH A MANNER AS TO ASSURE SAFETY AND SECURITY. LIGHTING SOURCES SHALL BE SHIELDED AND THERE SHALL BE NO SPILLOVER ONTO ADJACENT PROPERTIES OR RIGHT OF WAY.
13.

CONTRACTOR TO SUBMIT AS-BUILT PLANS TO THE DRAINAGE ENGINEER, DEPARTMENT OF PUBLIC WORKS WITHIN TEN DAYS OF PROJECT COMPLETION. (WHEN STORM SEWER IS CONSTRUCTED).
14.


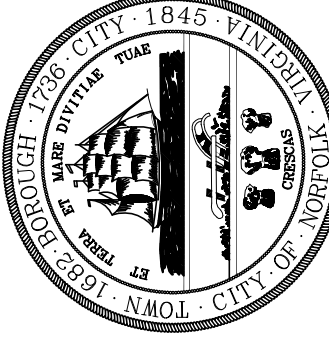
REMOVAL OR RELOCATION OF ALL UTILITIES AND APPURTENANCES WILL BE AT THE RISK AND EXPENSE OF THE CONTRACTOR, THESE INCLUDE, BUT ARE NOT LIMITED TO VIRGINIA POWER, SEWER, WATER, TELEPHONE, COMMUNICATIONS, CABLE, FIBER OPTICS, ETC.
15.

LOCATION AND DEPTH OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR WITH A CITY OF NORFOLK UTILITIES INSPECTOR PRESENT. CONTACT THE UTILITIES FIELD ENGINEERING OFFICE, 823-1001, AT LEAST 72 HOURS PRIOR TO EXCAVATION TO ARRANGE FOR AN INSPECTION.
16.

THE CONTRACTOR SHALL CONTACT THE CITY OF NORFOLK, BUREAU OF ENVIRONMENTAL SERVICES 664-4368 AT LEAST 48 HOURS PRIOR TO ANY LAND DISTURBING ACTIVITY SO THAT A PRECONSTRUCTION CONFERENCE CAN BE SCHEDULED.
17.

AS-BUILTS OF ANY IMPROVEMENTS RELATED TO STORMWATER AND STREETScape IS REQUIRED TO BE SUBMITTED TO PUBLIC WORKS – ENVIRONMMENTAL STORMWATER FOR REVIEW AND APPROVAL PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY. FAILURE TO PROVIDE THE NECESSARY AS-BUILT INFORMATION WILL RESULT IN DELAYS. AS-BUILTS ARE REQUIRED TO BE PERFORMED AND SEALED BY A LISCENSEED SURVEYOR SHOWING THE AS-BUILT LOCATION AND ELEVATIONS COMPARED TO THE PROPOSED.

FILE NAME: P:\6163-Norfolk PW\6163.08-Lafayette Blvd\Coa\2-CN-2.0 General Notes.dwg LAYOUT NAME: Layout1 PLOTTED: Friday, June 03, 2016 -- 9:55am



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

GENERAL NOTES

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE

PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CN-2.0

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SURVEY LEGEND:

SYMBOL	ABBREV.	DESCRIPTION	SYMBOL	ABBREV.	DESCRIPTION
	AC.	ACRES		MW	MONITORING WELL
	AATUR	ABANDONED ACCORDING TO UTILITY RECORDS		N/F	NOW OR FORMERLY
	BIT.	BITUMINOUS		(OA)	OVERALL
	BOLLARD	BOLLARD		PED.	PEDESTAL
	BORING	BORING		PLUG/STUB	PLUG/STUB
	BRUSH OR DRIPLINE	BRUSH OR DRIPLINE		P.O.B.	POINT OF BEGINNING
	SBL	BUILDING SETBACK LINE		P.C.	POINT OF CURVE
	CATV	CABLE TELEVISION		PVC	POLYVINYL CHLORIDE
	CI	CAST IRON		RR	RAILROAD
	CL	CENTERLINE		RCP	REINFORCED CONCRETE PIPE
	CLD	CENTERLINE OF DITCH		R/W, ROW	RIGHT-OF-WAY
	OHW	CENTERLINE OVERHEAD WIRES		RDCO	ROOF DRAIN CLEANOUT
	C&G	CONCRETE CURB & GUTTER		RFMV	SANITARY FORCE MAIN VALVE
	CMP	CORRUGATED METAL PIPE		--SFM--	SANITARY FORCEMAIN
	CB	CURB BASIN		--SS--	SANITARY SEWER LINE
	DATUR	DEPICTED ACCORDING TO UTILITY RECORDS		SSCO	SANITARY SEWER CLEANOUT
	DI	DROP INLET		SSMH	SANITARY SEWER MANHOLE
	EASEMENT LINE	EASEMENT LINE		(S)	SET MONUMENT
	EP	EDGE OF PAVEMENT		SIGN	SIGN
	EW	EDGE OF WATER		SPOT ELEVATION	SPOT ELEVATION
	ELEC.	ELECTRIC		S.F.	SQUARE FEET
	EMH	ELECTRIC MANHOLE		S.H.P.B.	STATE HIGHWAY PLAT BOOK
	ELEV, EL	ELEVATION		SDMH	STORM DRAIN MANHOLE
	EMERGENCY PUMP CONNECTION	EMERGENCY PUMP CONNECTION		SD	STORM DRAIN PIPE
	EOI	END OF INFORMATION		STRUC.	STRUCTURE
	EXCEPTION ITEM DESIGNATOR	EXCEPTION ITEM DESIGNATOR		SURVEY CONTROL	SURVEY CONTROL
	FENCE	FENCE		MON.	SURVEY MONUMENT
	FF	FINISHED FLOOR ELEVATION		TELE.	TELEPHONE
	FH	FIRE HYDRANT		TMH	TELEPHONE MANHOLE
	FP	FLAG POLE		TBM	TEMPORARY BENCHMARK
	FES	FLARED END SECTION		TOE OF SLOPE/BANK	TOE OF SLOPE/BANK
	FIRM	FLOOD INSURANCE RATE MAP		TOP OF SLOPE/BANK	TOP OF SLOPE/BANK
	FL	FLOW LINE		TC	TOP OF CURB
	(F)	FOUND MONUMENT		RIM	TOP OF STRUCTURE ELEVATION
	GM	GAS METER		TRAF.	TRAFFIC
	GV	GAS VALVE		TSP	TRAFFIC SIGNAL POLE

GPIN	GEOGRAPHIC PARCEL IDENTIFICATION NUMBER	TRANS.	TRANSFORMER
	GUY WIRE		TREE TRUNK
	HC		UNDERGROUND CABLE TELEVISION
	HVAC		UNDERGROUND ELECTRIC
	HDPE		UNDERGROUND NATURAL GAS
	INSTR.		UNDERGROUND TELEPHONE
	INV		UNDERGROUND TRAFFIC CONTROL
	JBOX		UTILITY POLE
	LANDSCAPED AREA		UTILITY POLE W/LAMP
	LP		WATER VALVE
	LOT LINE		VAULT
	MAIL BOX		WATER LINE
	MH		WATER METER
	MARSH		WETLANDS FLAG

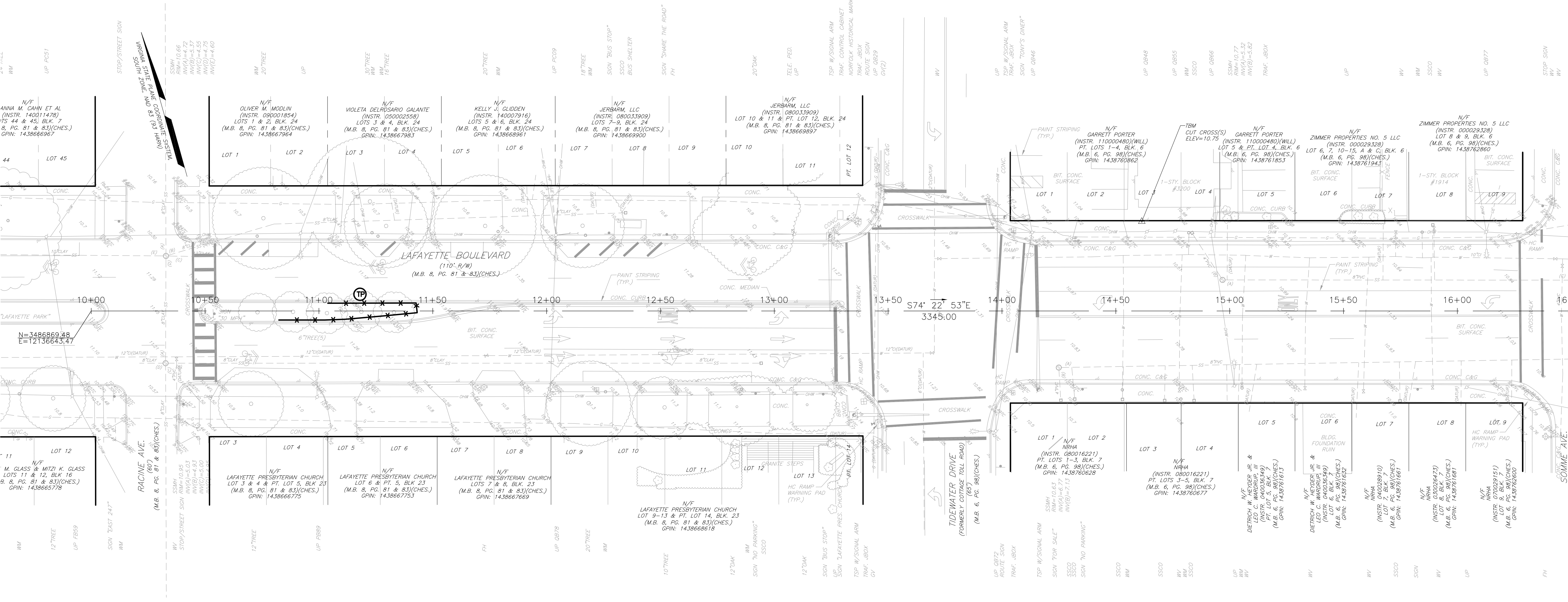
SURVEY NOTES:

- NORFOLK GEODETIC CONTROL NETWORK WHICH REFERS TO THE VIRGINIA STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NAD 83 (93 HARN).
- S-2. ELEVATIONS SHOWN ARE EXPRESSED IN FEET AND BASED ON THE CITY OF NORFOLK GEODETIC CONTROL NETWORK WHICH REFERS TO NAVD 88. CONTROL STATION UTILIZED: A3 ELEV=10.65.
- S-3. THIS SURVEY/PLAT WAS PERFORMED/PREPARED WITHOUT THE BENEFIT OF A CURRENT AND COMPLETE TITLE REPORT AND MAY NOT SHOW ANY/ALL PARCEL LINES, EASEMENTS, RESTRICTIONS, ENCUMBRANCES, AND CURRENT RIGHTS-OF-WAY THAT MAY AFFECT THE SURVEYED AREA SHOWN.
- S-4. THIS SURVEY/PLAT DOES NOT INTEND TO DEPICT ANY WETLANDS, HAZARDOUS WASTE, AND OTHER ENVIRONMENTAL CONDITIONS UNLESS OTHERWISE NOTED AND/OR SHOWN.
- S-5. THE UNDERGROUND UTILITY INFORMATION IS DEPICTED BASED ON A COMBINATION OF EXISTING DESIGNATIONS/MARKINGS, FIELD LOCATED STRUCTURES, AND PLAN INFORMATION. THEREFORE, THE UTILITY INFORMATION SHOWN MAY NOT ACCURATELY REPRESENT THAT OF WHICH IS ACTUALLY IN PLACE IN THE FIELD. THIS SURVEY/PLAT MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITY INFORMATION SHOWN COMPRISES ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED.
- S-6. THE PROPERTY SHOWN APPEARS TO FALL WITHIN FLOOD ZONE(S) "X", ACCORDING TO F.E.M.A.'S FLOOD INSURANCE RATE MAP (F.I.R.M.) FOR THE CITY OF NORFOLK, VIRGINIA, MAP NUMBER 5101040095F, MAP REVISED: SEPTEMBER 2, 2009.
- S-7. THIS TOPOGRAPHIC SURVEY OF A PORTION OF LAFAYETTE BLVD., NORFOLK, VIRGINIA WAS COMPLETED UNDER THE DIRECT AND RESPONSIBLE CHARGE OF CHARLES L. SMITH, III, LS FROM AN ACTUAL X GROUND OR AIRBORNE SURVEY MADE UNDER MY SUPERVISION; THAT THE IMAGERY AND/OR ORIGINAL DATA WAS OBTAINED ON 05/08/15; AND THAT THE PLAT, MAP, OR DIGITAL GEOSPATIAL DATA INCLUDING METADATA MEETS MINIMUM ACCURACY STANDARDS UNLESS OTHERWISE NOTED.

EROSION CONTROL LEGEND:

- (IP) INLET PROTECTION (STD. & SPEC. 3.07) (TS) TEMPORARY SEEDING (STD. & SPEC. 3.31)
- (TP) TREE PROTECTION (STD. & SPEC. 3.38) (PS) PERMANENT SEEDING (STD. & SPEC. 3.32)

FOR CONSTRUCTION DETAILS AND SPECIFICATIONS REFER TO THE "VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK" THIRD EDITION, 1992.



MATCHLINE- STATION 16+50, SEE SHEET CX-3.1 FOR CONTINUATION



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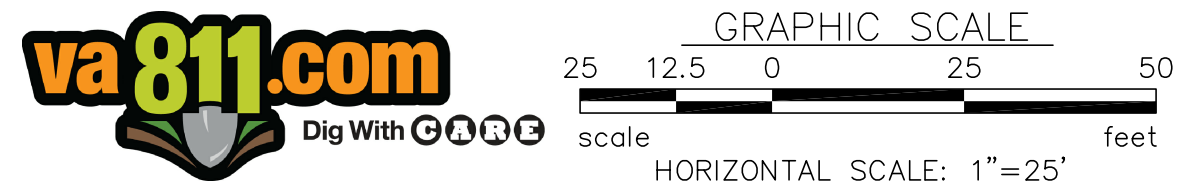
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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

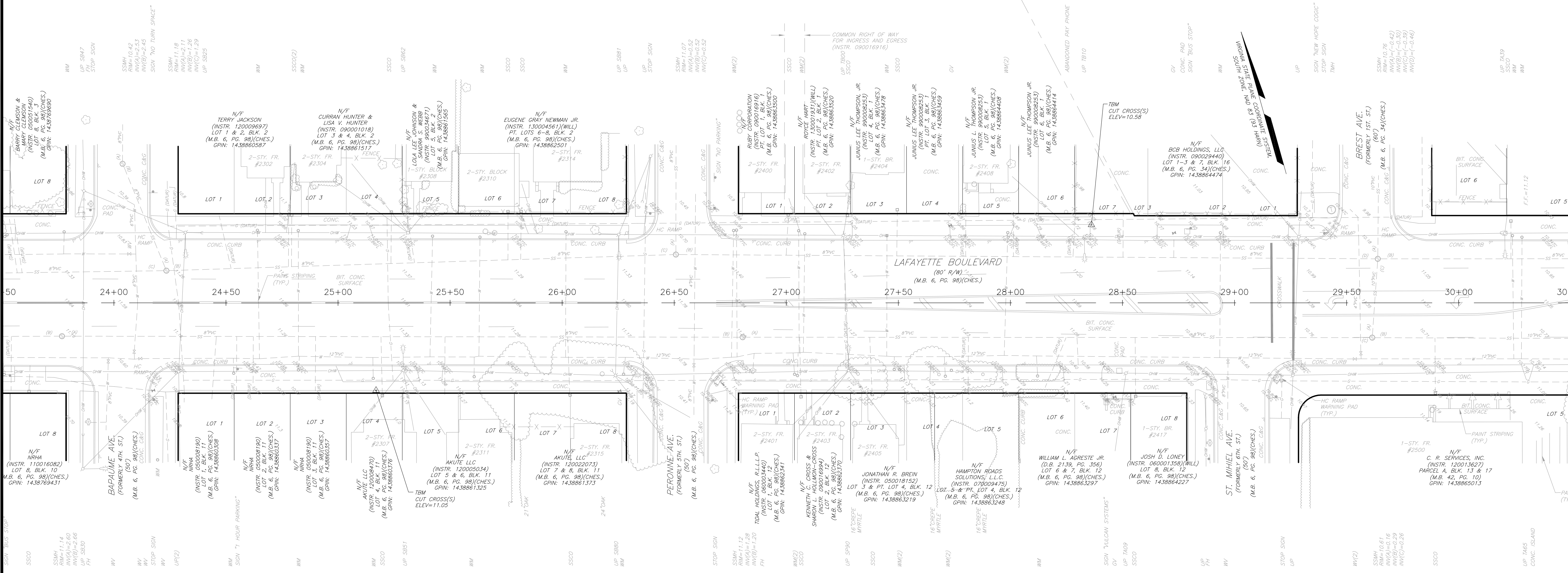
**EXISTING CONDITIONS AND
EROSION/SEDIMENT CONTROL PLAN**

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

DATE	5/24/16
PROJECT REVISIONS	
REV #	
DESIGNED BY:	E. TOLLE
PREPARED BY:	E. TOLLE
ISSUE DATE:	2/22/16
SHEET CX-3.0	



MATCHLINE- STATION 23+50, SEE SHEET CX-3.1 FOR CONTINUATION



MATCHLINE- STATION 30+50, SEE SHEET CX-3.3 FOR CONTINUATION

SEQUENCE OF CONSTRUCTION

- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES TO DEACTIVATE EXISTING UTILITIES PRIOR TO COMMENCING ANY DEMOLITION.
- CONTRACTOR SHALL SECURE ANY NECESSARY DEMOLITION PERMITS FROM THE CITY.
- PRIOR TO ANY DEMOLITION AND OR LAND DISTURBING ACTIVITIES, TREE PROTECTION FENCING, SILT FENCING, AND ALL ROOT PRUNING IS TO BE COMPLETE IF REQUIRED. ALL STORMWATER SYSTEM INCLUDING STRUCTURES, PIPING AND OUTFALL SHALL BE INSTALLED. THE STORM SYSTEM SHALL BE INSTALLED FROM THE DOWNSTREAM END AND EXTEND UPSTREAM.
- DEMOLISH EXISTING ITEMS IN ACCORDANCE WITH THE DEMOLITION PLAN, FOLLOWING ANY SAFETY PRECAUTIONS PROVIDED BY THE UTILITY COMPANIES.
- MILL EXISTING PAVEMENT, COMPLETE DEMOLITION OF PAVED AREAS AND CONCRETE CURBING AND DRIVEWAYS.
- ROUGH GRADE PROJECT LIMITS WHERE INDICATED, INSTALL CONCRETE CURBING, AND DRIVEWAYS, INSTALL NEW PAVEMENT SECTION.
- STABILIZE AND SEED DISTURBED AREAS AS SOON AS POSSIBLE.
- AFTER AREA IS STABILIZED, REMOVE SEDIMENT FROM INLET PROTECTION DEVICES AND SEED DISTURBED AREAS.
- CLEAN STORM DRAINAGE SYSTEMS OF ANY DEBRIS.
- REMOVE REMAINING TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES WITH THE APPROVAL OF THE CITY.
- ALL EROSION CONTROL DEVICES SHALL BE INSTALLED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH THE 1992 EDITION OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK AND SECTION 1.5 OF THE E&S REGULATIONS OF THE VIRGINIA DIVISION OF SOIL AND WATER CONSERVATION (VR 625-02-000).

EQUIPMENT OPERATION AND STORAGE: HEAVY EQUIPMENT, VEHICULAR TRAFFIC, OR STOCKPILES OF ANY CONSTRUCTION MATERIAL (INCLUDING TOPSOIL) SHALL NOT BE ALLOWED WITHIN THE DRIP LINES OF ANY TREE TO BE RETAINED. TREES BEING REMOVED SHALL NO FEELED, PUSHED OR PULLED INTO TREES BEING RETAINED.

A TREE PERMIT FROM PARKS AND URBAN FORESTRY WILL BE REQUIRED FOR ANY ROOT PRUNING, TREE TRIMMING, AND TREE REMOVAL OF THE CITY OF NORFOLK TREES.

CITY OF NORFOLK TREES MUST BE PROTECTED AND SHALL BE IN HEALTHY CONDITION AND UNDAMAGED OR TREES WILL NEED TO BE MITIGATED; AS DIRECTED BY THE CITY OF NORFOLK'S URBAN FORESTER.

ANY ROOT PRUNING, TREE TRIMMING, AND TREE REMOVAL MUST BE PERFORMED BY A CERTIFIED ARBORIST OF THE STATE OF VIRGINIA.

SEED
TOPSOIL
LIME
FERTILIZER
MULCH

PER VOLUME III OF THE PFM
4"THICK, PER V.D.O.T. SPEC.602/CLASS B)
4000#/AC.
1000#/AC. OF 10-10-10
2000#/AC.

EROSION AND SEDIMENT CONTROL NOTES:

MANAGEMENT STRATEGIES AND SEQUENCES OF EROSION CONTROL MEASURES

THE CONTRACTOR SHALL COMPLY IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE APPLICABLE REGULATIONS OF THE CITY OF NORFOLK "EROSION & SEDIMENT CONTROL STANDARDS AND SPECIFICATIONS" AND THE LATEST EDITION OF THE STATE OF VIRGINIA EROSION & SEDIMENT CONTROL HANDBOOK BEFORE, DURING, AND AFTER CONSTRUCTION. THE FOLLOWING SEQUENCE OF EVENTS AND EROSION CONTROL MEASURES SHALL BE INCORPORATED INTO THE CONSTRUCTION SCHEDULE FOR THIS PROJECT AND SHALL APPLY TO ALL CONSTRUCTION ACTIVITIES WITHIN THE PROJECT LIMITS:

- A) TEMPORARY CONSTRUCTION ENTRANCE(S) SHALL BE PROVIDED AT THE LOCATION(S) SHOWN ON THE PLANS. THIS ENTRANCE(S) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE VIRGINIA EROSION & SEDIMENT CONTROL HANDBOOK (STD. & SPEC. 3.02). WASHRACKS ARE TO BE PROVIDED WHERE WATER IS AVAILABLE.
B) WHERE CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT ONTO THE PAVED SURFACE. WHERE SEDIMENT IS TRANSPORTED ONTO A PUBLIC ROAD SURFACE, THE ROAD SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM THE ROAD BY SHOVELING OR SWEEPING AND TRANSPORTED TO A SEDIMENT DISPOSAL AREA. STREET WASHING SHALL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER. THIS PROVISION SHALL APPLY TO INDIVIDUAL SUBDIVISION LOTS AS WELL AS LARGER LAND DISTURBING ACTIVITIES.
C) CONSTRUCTION TRAFFIC SHALL BE LIMITED TO ACCESS ROADS. ALL TRAFFIC IS PROHIBITED FROM CROSSING DRAINAGE SWALES AND STREAMS EXCEPT WHERE ABSOLUTELY NECESSARY (STD. & SPEC. 3.24 VE&SC HANDBOOK)
- DEWATERING AND WELL POINT DISCHARGE OPERATIONS MUST PROVIDE APPROPRIATE EROSION CONTROL DEVICES AND PRACTICES AND BE APPROVED BY THE APPROPRIATE INSPECTIONS BUREAU PRIOR TO COMMENCEMENT OF DISCHARGE OPERATIONS. FAILURE TO COMPLY MAY RESULT IN A "STOP WORK ORDER" BEING ISSUED.
- TEMPORARY SEDIMENT TRAPS, SEDIMENT BARRIERS, CONSTRUCTION ENTRANCE, AND EROSION CONTROL STONE ARE TO BE PLACED PRIOR TO CLEARING AND GRUBBING PRIOR TO THE FIRST PHASE OF CONSTRUCTION.
- ALL PERMANENT STORM WATER MANAGEMENT FACILITIES INCLUDING EROSION CONTROL ARE TO BE INSTALLED AND MADE OPERATIONAL AT THE START OF CLEARING OPERATIONS, INCLUDING APPROVED SEDIMENT BASINS.
- THE CONTRACTOR SHALL COMPLETE DRAINAGE FACILITIES WITHIN THIRTY (30) DAYS FOLLOWING COMPLETION OF ROUGH GRADING AT ANY POINT WITHIN THE PROJECT.
- CONSTRUCTION WILL BE SEQUENCED SO THAT GRADING OPERATIONS CAN BEGIN AND END AS QUICKLY AS POSSIBLE.
- AREAS WHICH ARE NOT TO BE DISTURBED WILL BE CLEARLY MARKED BY FENCING, FLAGS, SIGNS, ETC.

A) PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN (7) DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED WITHIN SEVEN (7) DAYS TO DENUDED AREAS THAT MAY NOT BE AT FINAL GRADE BUT WILL REMAIN DORMANT (UNDISTURBED) FOR LONGER THAN THIRTY (30) DAYS. PERMANENT STABILIZATION SHALL BE APPLIED TO AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN ONE (1) YEAR.

B) DURING CONSTRUCTION OF PROJECT, SOIL STOCKPILES SHALL BE STABILIZED OR PROTECTED WITH SEDIMENT TRAPPING MEASURES. THE APPLICANT IS RESPONSIBLE FOR THE TEMPORARY PROTECTION AND PERMANENT STABILIZATION OF ALL SOIL STOCKPILES ON SITE AS WELL AS SOIL INTENTIONALLY TRANSPORTED FROM THE PROJECT SITE.

C) A PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED ON DENUDED AREAS NOT OTHERWISE PERMANENTLY STABILIZED. PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL A GROUND COVER IS ACHIEVED THAT, IN THE OPINION OF THE LOCAL PROGRAM ADMINISTRATOR OR HIS DESIGNATED AGENT IS UNIFORM, MATURE ENOUGH TO SURVIVE AND WILL INHIBIT EROSION. VEGETAL COVER SHALL BE ESTABLISHED AS FOLLOWS:

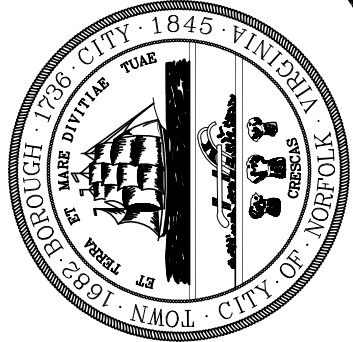
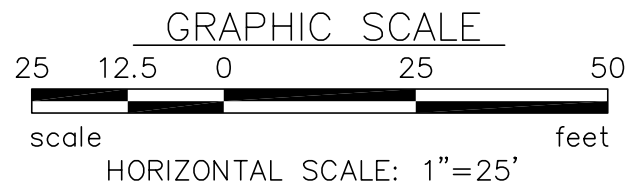
(HYDROSEEDING MAY BE USED IN PLACE OF MULCHING ON AREA OTHER THAN DITCH BANKS). STABILIZATION MEASURES SHALL BE APPLIED TO EARTHEN STRUCTURES SUCH AS DAMS, DIVERSIONS, AND DITCH OR WATERCOURSE BEDS AND BANKS IMMEDIATELY AFTER INSTALLATION (STD. & SPEC. 33.6 VE&SC HANDBOOK).

- A) ALL STORM SEWER INLETS THAT ARE MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT SEDIMENT LADEN WATER CANNOT ENTER THE CONVEYANCE SYSTEM WITHOUT FIRST BEING FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT.
B) BEFORE NEWLY CONSTRUCTED CONVEYANCE CHANNELS ARE MADE OPERATIONAL, ADEQUATE OUTLET PROTECTION AND ANY REQUIRED TEMPORARY OR PERMANENT CHANNEL LINING SHALL BE INSTALLED IN BOTH THE CONVEYANCE CHANNEL AND RECEIVING CHANNEL.
- A) CUT AND FILL SLOPES SHALL BE DESIGNED AND CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. SLOPES THAT ARE FOUND TO BE ERODING EXCESSIVELY WITHIN ONE (1) YEAR OF PERMANENT STABILIZATION SHALL BE PROVIDED WITH ADDITIONAL SLOPE STABILIZING MEASURES UNTIL THE PROBLEM IS CORRECTED.
B) CONCENTRATED RUNOFF SHALL NOT FLOW DOWN CUT OR FILL SLOPES UNLESS CONTAINED WITHIN AN ADEQUATE TEMPORARY OR PERMANENT CHANNEL, FLUME, OR SLOPE DRAIN STRUCTURE.
- PERIODIC INSPECTIONS AND REQUIRED MAINTENANCE MUST BE PROVIDED, ESPECIALLY AFTER EACH SIGNIFICANT STORM. THE PROJECT SUPERINTENDENT SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION & SEDIMENT CONTROL PRACTICES.
- THE CONTRACTOR SHALL MONITOR AND TAKE PRECAUTIONS TO CONTROL DUST, INCLUDING (BUT NOT LIMITED TO) USE OF WATER OR CHEMICAL DUST PALLIATIVE, LIMITING THE NUMBER OF VEHICLES ALLOWED ON-SITE, MINIMIZING THE OPERATING SPEED OF ALL VEHICLES, ETC.
- RIPRAP SHALL BE PLACED AT THE NORMAL WATER ELEVATION OF THE PROPOSED WET POND TO PREVENT EROSION OR AS DIRECTED BY THE CIVIL INSPECTOR AS NECESSARY.
- THE PLAN APPROVING AUTHORITY MUST BE NOTIFIED ONE (1) WEEK PRIOR TO THE PRECONSTRUCTION CONFERENCE, ONE (1) WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITY, AND ONE (1) WEEK PRIOR TO THE FINAL INSPECTION.

ROOT PRUNING NOTES:

(SEE SHEET CX-3.4 FOR ROOT PRUNING DETAILS)

- ROOT PRUNING SHALL BE DONE WITH A TRENCHER OR VIBRATORY FLOW TO A DEPTH OF 18". ROOTS OVER 1.5" IN DIAMETER SHALL HAVE A CLEAN CUT MADE BY A CLEAN SAW ON THE SURFACE OF THE ROOT WHICH IS STILL ATTACHED TO THE TREE. DO NOT PAINT THE CUT ROOT END. IF EXCAVATION IS FOR INSTALLATION OF UNDERGROUND UTILITIES. LEAVE THE ROOT INTACT AND THREAD THE LINES UNDERNEATH.
- ROOT PRUNING SHALL TAKE PLACE PRIOR TO ANY CLEARING AND GRADING. EXACT LOCATION OF TREE PROTECTION AREAS SHALL BE STAKED OR FLAGGED PRIOR TO TRENCHING.
- ROOT PRUNING SHALL BE CONDUCTED WITH THE SUPERVISION OF A CERTIFIED ARBORIST.
- BACKFILL THE ROOT-PRUNING TRENCH WITH EXCAVATED SOIL AND MULCH AND MARK THE LOCATION FOR FUTURE REFERENCE. SILT FENCE MAY BE INSTALLED IN TRENCH PRIOR TO BACKFILLING LONG AS THE TRENCH IS NOT OPEN FOR LONGER THAN 48 HOURS WITHOUT WATERING.
- ROOT PRUNING WORK WILL NOT BE DONE WHEN MORE THAN THE TOP 1 INCH OF SOIL IS FROZEN. ROOT PRUNING WILL NOT BE UNDERTAKEN WHEN THE SOIL IS WET AND CONDITIONS ARE MUDDY.
- THE CITY ARBORIST SHALL BE NOTIFIED WHEN ALL ROOT PRUNING AND TREE PROTECTION FENCE INSTALLATION IS COMPLETE.



CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
EXISTING CONDITIONS AND
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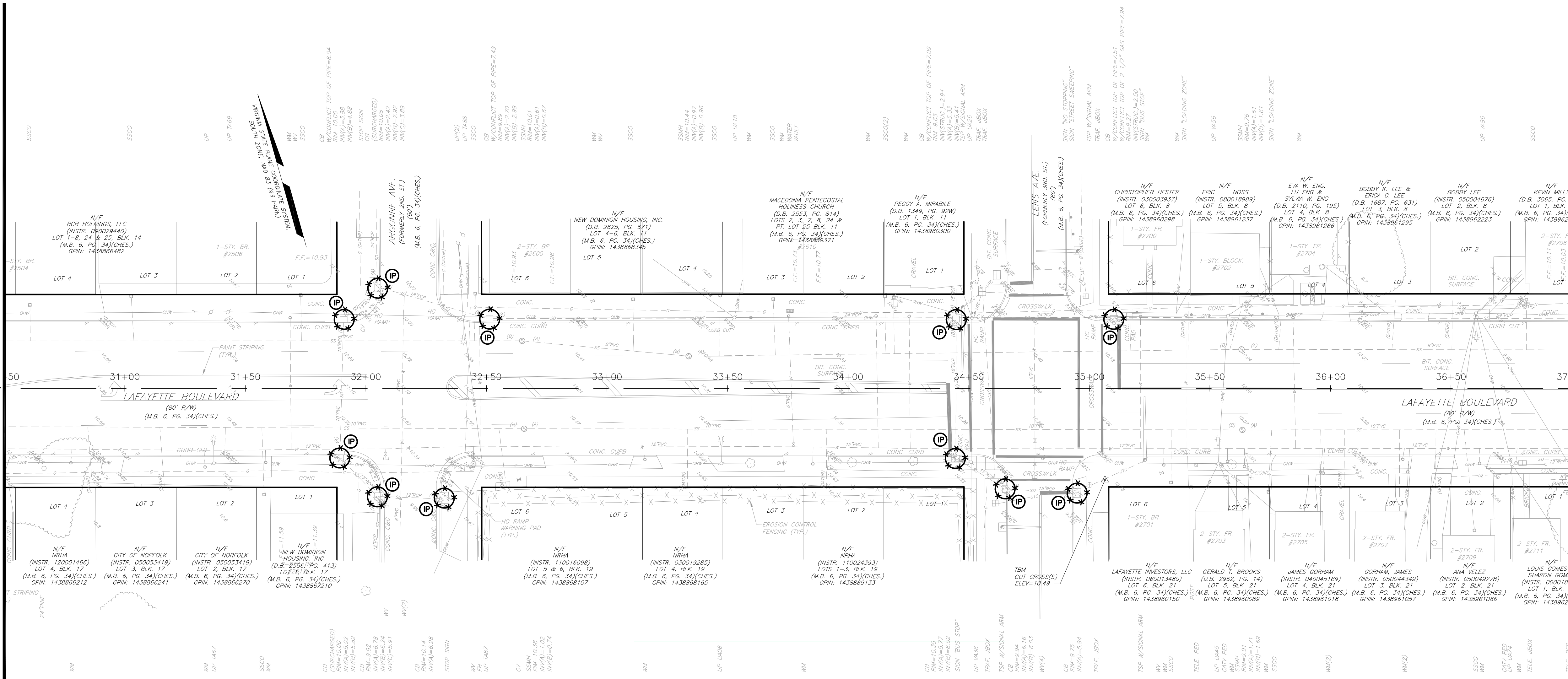
REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CX-3.2

MATCHLINE- STATION 30+50, SEE SHEET CX-3.2 FOR CONTINUATION



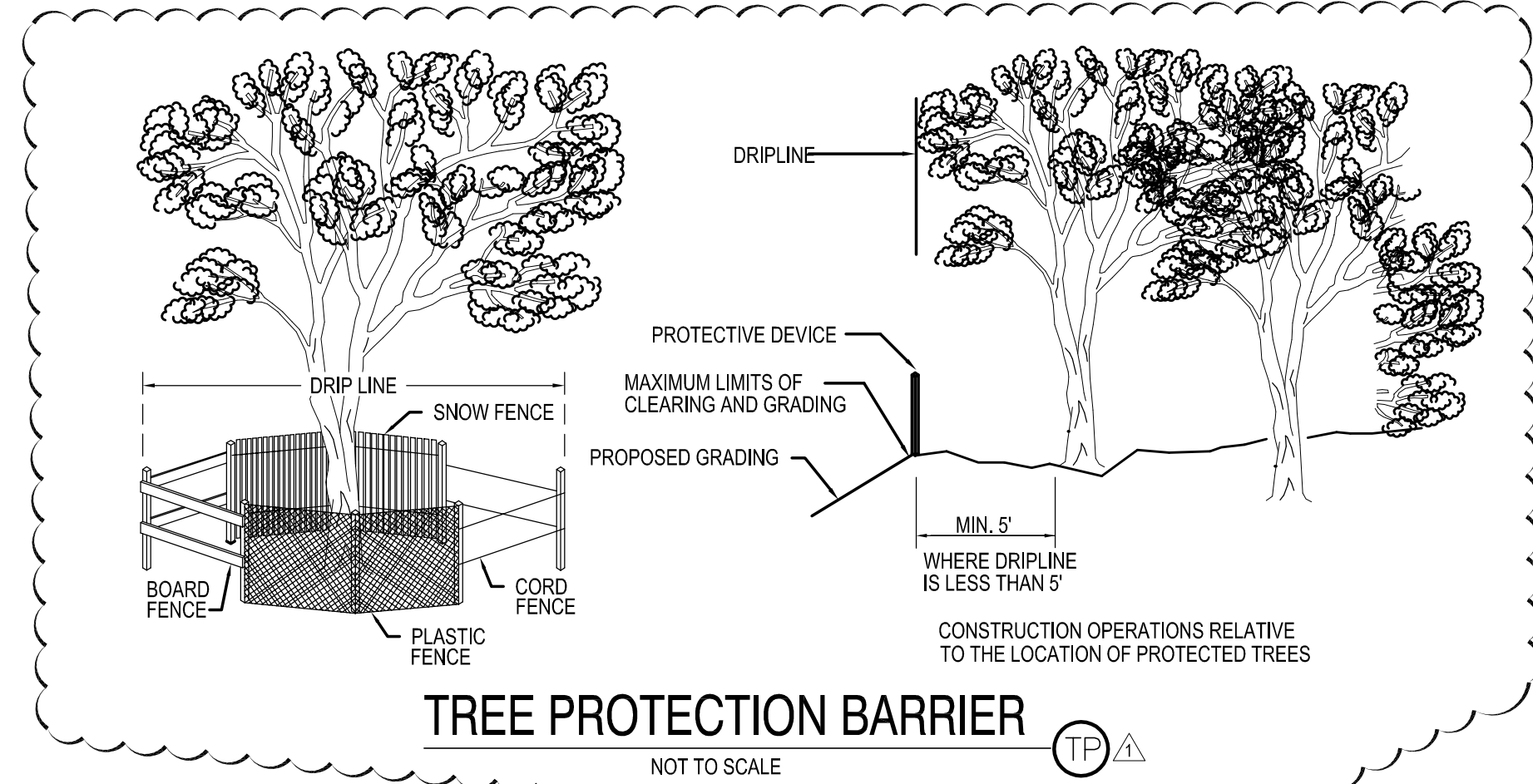
MATCHLINE- STATION 37+00, SEE SHEET CX-3.4 FOR CONTINUATION

NORFOLK STANDARD E&S NOTES:

- UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED AND MAINTAINED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK (3RD EDITION, 1992) AND THE CITY OF NORFOLK EROSION AND SEDIMENT CONTROL ORDINANCE.
- THE CONTRACTOR SHALL CONTACT THE CITY OF NORFOLK, BUREAU OF ENVIRONMENTAL SERVICES (664-4368) AT LEAST 48 HOURS PRIOR TO ANY LAND DISTURBING ACTIVITY (INCLUDING DEMOLITION) SO THAT A PRECONSTRUCTION CONFERENCE CAN BE SCHEDULED.
- THE CONTRACTOR SHALL APPLY PERMANENT OR TEMPORARY SOIL STABILIZATION TO ALL DENUDED OR DISTURBED AREAS WITHIN 7 DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. SOIL STABILIZATION MUST ALSO BE APPLIED TO DENUDED OR DISTURBED AREAS WHICH MAY NOT BE AT FINAL GRADE BUT WHICH WILL REMAIN UNDISTURBED FOR LONGER THAN 14 DAYS. SOIL STABILIZATION MEASURES INCLUDE VEGETATIVE ESTABLISHMENT, MULCHING AND THE EARLY APPLICATION OF GRAVEL BASE MATERIAL ON AREAS TO BE PAVED.
- ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO OR AS THE FIRST STEP IN CONSTRUCTION.
- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES AND CLEANUP OF SEDIMENTATION ARE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE MADE IMMEDIATELY.
- THE CONTRACTOR SHALL LIMIT SITE ACCESS BY CONSTRUCTION VEHICLES TO ENTRANCES PROTECTED BY A STONE CONSTRUCTION ENTRANCE (VESCH STD. & SPEC. 3.02) OR AN APPROVED COMPARABLE CONTROL MEASURE. SEDIMENT SHALL BE REMOVED FROM PAVED AREAS ON A DAILY BASIS.
- STOCK PILES OF SOIL AND OTHER ERODIBLE MATERIALS SHALL BE STABILIZED OR PROTECTED WITH SEDIMENT TRAPPING MEASURES. THE CONTRACTOR IS RESPONSIBLE FOR THE TEMPORARY PROTECTION AND PERMANENT STABILIZATION FOR STOCKPILES ON SITE AS WELL AS FOR MATERIALS TRANSPORTED FROM THE PROJECT SITE.
- THE CONTRACTOR SHALL MONITOR AND TAKE PRECAUTIONS TO CONTROL DUST INCLUDING (BUT NOT LIMITED TO) USE OF WATER, MULCH, OR CHEMICAL DUST ADHESIVES AND CONTROL OF CONSTRUCTION SITE TRAFFIC.
- EFFLUENT FROM DE-WATERING OPERATIONS SHALL BE FILTERED OR PASSED THROUGH AN APPROVED SEDIMENT TRAPPING DEVICE, OR BOTH, AND DISCHARGED IN A MANNER THAT DOES NOT ADVERSELY AFFECT ADJACENT PROPERTIES, WETLANDS, WATERWAYS OR THE STORM DRAINAGE SYSTEM.
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION AND MAINTENANCE OF ANY ADDITIONAL CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED NECESSARY BY THE PLAN APPROVING AUTHORITY.
- TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES ARE NOT TO BE REMOVED UNTIL ALL DISTURBED AREAS ARE STABILIZED. AFTER STABILIZATION IS COMPLETE, ALL MEASURES SHALL BE REMOVED WITHIN 30 DAYS. TRAPPED SEDIMENT SHALL BE SPREAD AND SEEDED.
- CONTRACTOR SHALL HAND DIG AND FOLLOW ROOT PRUNING REQUIREMENTS WITHIN DRIP LINE OF EXISTING TREES. COORDINATE WITH CITY ARBORIST FOR MORE DETAILS.

TABLE 3.32-E (Revised June 2003) PERMANENT SEEDING SPECIFICATIONS FOR COASTAL PLAIN AREA		
LAND USE	SEED ¹ SPECIES	APPLICATION RATES
Minimum Care Lawn, (Commercial or Residential)	Tall Fescue ¹ or Bermudagrass ¹	175 - 200 lbs. 75 lbs.
High-Maintenance Lawn	Tall Fescue ¹ or Bermudagrass ¹ (seed) or Bermudagrass ¹ (by other vegetative establishment method, see Std. & Spec. 3.34)	200-250 lbs. 40 lbs. (unhulled) 30 lbs. (hulled)
General Slope (3:1 or less)	Tall Fescue ¹ Red Top Grass or Creeping Red Fescue Seasonal Nurse Crop ²	128 lbs. 2 lbs. 20 lbs. TOTAL: 150 lbs.
Low-Maintenance Slope, (Sloeper than 3:1)	Tall Fescue ¹ Bermudagrass ¹ Red Top Grass or Creeping Red Fescue Seasonal Nurse Crop ² Sericea Lespedeza ³	63-108 lbs. 0-15 lbs. 2 lbs. 20 lbs. 20 lbs. TOTAL: 150 lbs.
1 - When selecting varieties of turfgrass, use the Virginia Crop Improvement Association (VCIA) recommended turfgrass variety list. Quality seed will bear a label indicating that they are approved by VCIA. A current turfgrass variety list is available at the local County Extension office or through VCIA at 804-746-4884 or at http://suden.ces.vt.edu/html/TurfTurfPublications/publications2.html		
2 - Use seasonal nurse crop in accordance with seeding dates as stated below:		
February, March - April..... Annual Rye		
May 1 st - August..... Foxtail Millet		
September, October - November 1 st Annual Rye		
November 16 th - January..... Winter Rye		
3 - May through October, use hulled seed. All other seeding periods, use unhulled seed. If Weeping Lovegrass is used, include in any slope or low maintenance mixture during warmer seeding periods, increase to 30 -40 lbs/acre.		
FERTILIZER & LIME		
• Apply 10-20-10 fertilizer at a rate of 500 lbs. / acre (or 12 lbs. / 1,000 sq. ft.)		
• Apply Pulverized Agricultural Limestone at a rate of 2 tons/acre (or 90 lbs. / 1,000 sq. ft.)		
NOTE: - A soil test is necessary to determine the actual amount of lime required to adjust the soil pH of site. - Incorporate the lime and fertilizer into the top 4 - 6 inches of the soil by disk or by other means. - When applying Slowly Available Nitrogen, use rates available in Erosion & Sediment Control Technical Bulletin # 4, 2003 Nutrient Management for Development Sites at http://www.dcr.state.va.us/sw/e&s.htm#pubs		

TABLE 3.31-B (Revised June 2003) TEMPORARY SEEDING SPECIFICATIONS QUICK REFERENCE FOR ALL REGIONS		
APPLICATION DATES	SPECIES	APPLICATION RATES
Sept. 1 - Feb. 15	50/50 Mix of Annual Ryegrass (toilum multi-florum) & Cereal (Winter) Rye (Secale cereale)	50 -100 (lbs/acre)
Feb. 16 - Apr. 30	Annual Ryegrass (toilum multi-florum)	60 - 100 (lbs/acre)
May 1 - Aug. 31	German Millet	50 (lbs/acre)
FERTILIZER & LIME		
• Apply 10-10-10 fertilizer at a rate of 450 lbs. / acre (or 10 lbs. / 1,000 sq. ft.)		
• Apply Pulverized Agricultural Limestone at a rate of 2 tons/acre (or 90 lbs. / 1,000 sq. ft.)		
NOTE: 1 - A soil test is necessary to determine the actual amount of lime required to adjust the soil pH of site. 2 - Incorporate the lime and fertilizer into the top 4 - 6 inches of the soil by disk or by other means. 3 - When applying Slowly Available Nitrogen, use rates available in Erosion & Sediment Control Technical Bulletin # 4, 2003 Nutrient Management for Development Sites at http://www.dcr.state.va.us/sw/e&s.htm#pubs		

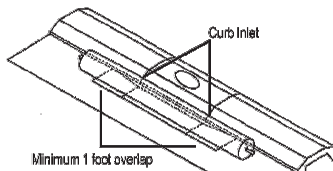


Gutterbuddy® Specification For Curb Gutter Storm Drains

1.0 Description

- This work shall consist of furnishing, placing, maintaining and removing the Gutterbuddy® sediment control device as directed by the engineer and as shown on the contract drawings. The Gutterbuddy® sediment control system distributed by:

ACF Environmental, Inc.
2831 Cornwall Road
Richmond, Virginia 23234
Phone: 804-448-3038 • Fax: 804-743-7779
www.acfenvironmental.com



2.0 Materials

2.1 GUTTERBUDDY®

The Gutterbuddy® shall be synthetic filter manufactured from recycled synthetic fibers.

2.1.1 The Gutterbuddy® will be manufactured to be 9" in diameter and are available in 4', 6', 8', 10', 12', 14' and 16' lengths and a minimum of twenty-four (24) inches longer than the curb inlet opening. This will allow for sufficient length to cover the inlet with twelve (12) inches beyond the inlet on both ends.

3.0 Construction Sequence

3.1 General

- Install the Gutterbuddy® in front of the curb inlet opening. Each end of the Gutterbuddy® should overlap the curb inlet approximately 12".

- The Gutterbuddy® should be cleaned if a visual inspection shows silt and debris build up around the Gutterbuddy®.

- To remove the Gutterbuddy®, lift out of the opening.

- The Gutterbuddy® is reusable. Once the construction project is complete and it is no longer needed for sediment control, remove, clean and store out of the sunlight until needed on the next project.

- Ponding is likely if sediment is not removed regularly. Inspection of Gutterbuddy® should be on a regular basis and immediately after major rain events.

4.0 Basis of Payment

- The payment for any Gutterbuddy® used during the construction is to be included in the bid of the overall erosion and sediment control plan and priced by the linear foot.

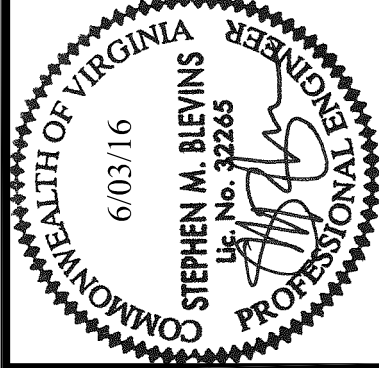
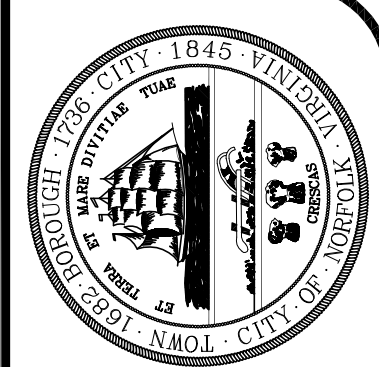
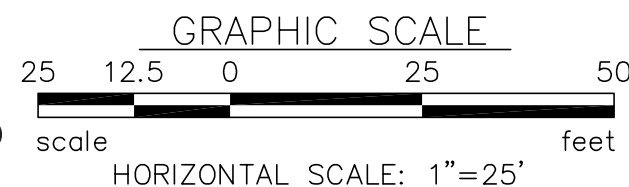


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Richmond, Virginia 23234
(804) 448-3038 • Fax: (804) 743-7779
www.acfenvironmental.com

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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

**EXISTING CONDITIONS AND
EROSION/SEDIMENT CONTROL PLAN**

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

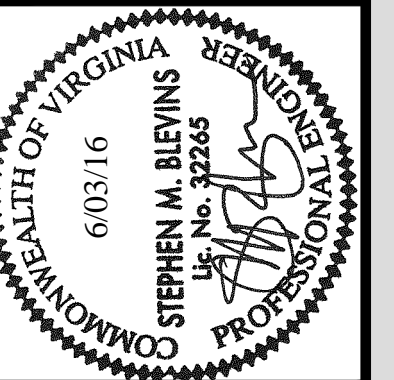
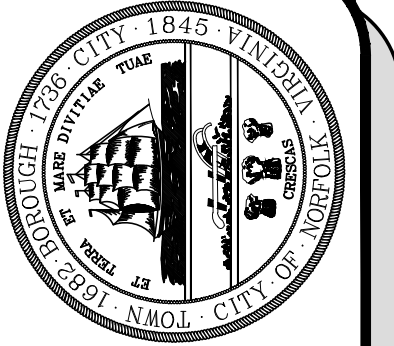
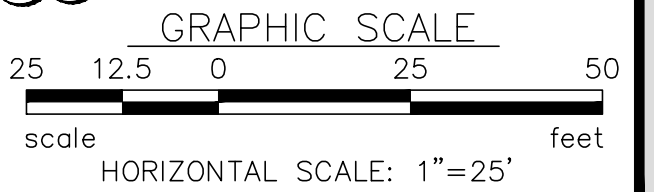
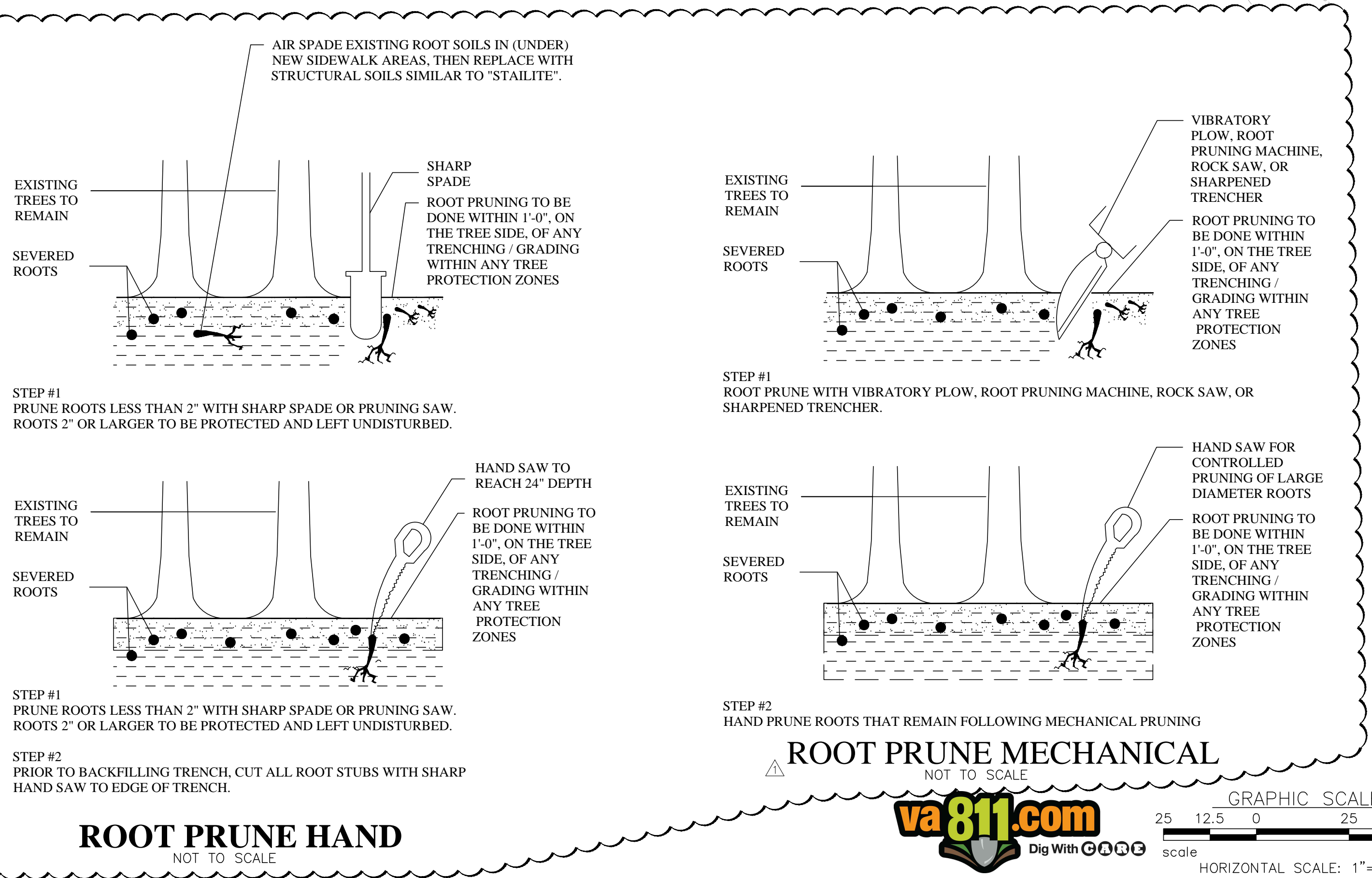
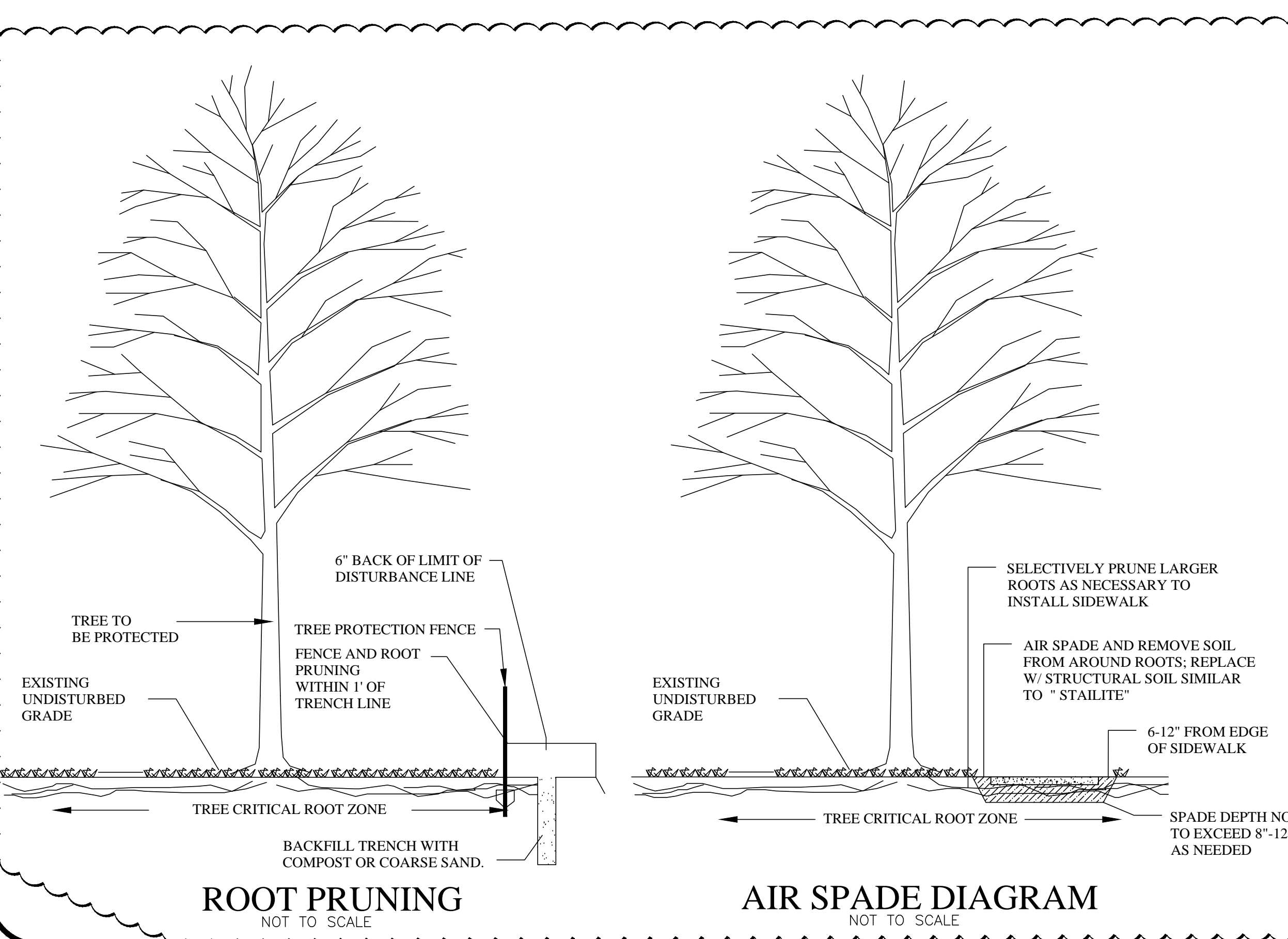
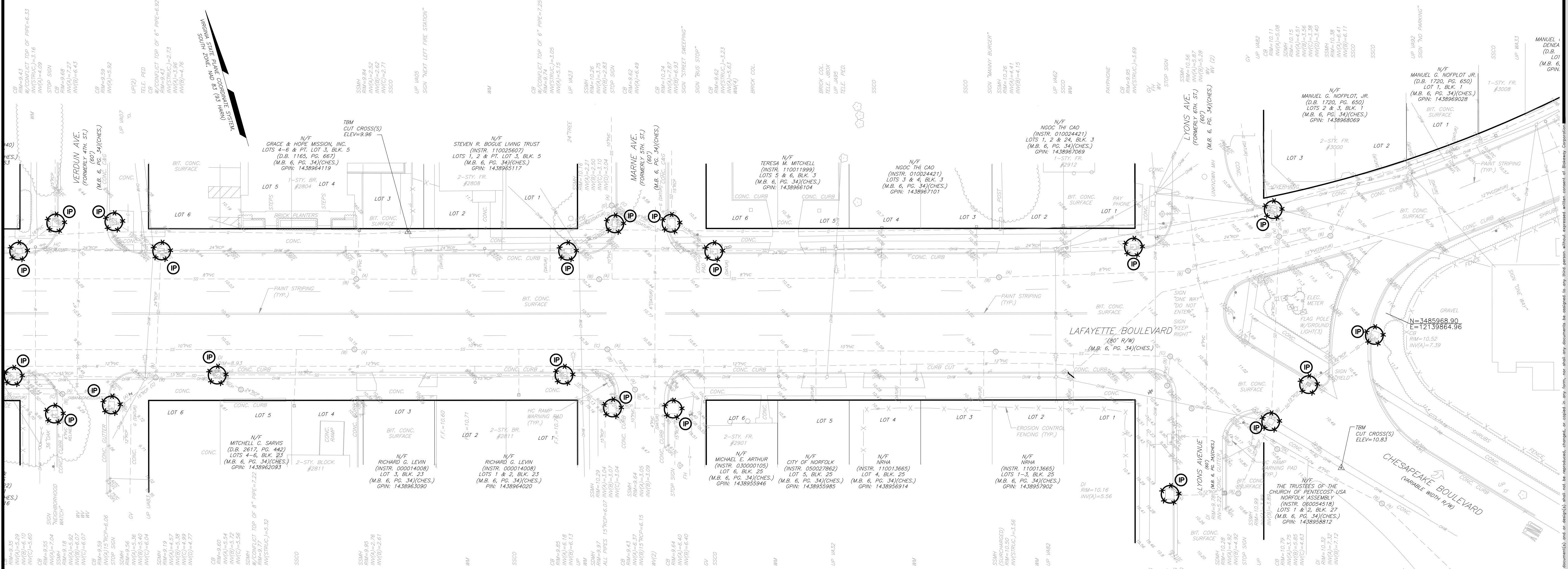
REV #	DATE	PROJECT REVISIONS
1	5/24/16	REVISED PER CITY COMMENTS DATED 3/3/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CX-3.3

MATCHLINE- STATION 37+00, SEE SHEET CX-3.3 FOR CONTINUATION



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

EXISTING CONDITIONS AND EROSION/SEDIMENT CONTROL PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED PER CITY COMMENTS DATED 3/3/16	5/24/16
2		
3		
4		
5		
6		
7		
8		
9		
10		

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE: 2/22/16

SHEET CX-3.4

DEMOLITION LEGEND:

- 1 SAWCUT AND REMOVE EXISTING CONCRETE DRIVEWAY OR SIDEWALK
- 2 SAWCUT AND REMOVE EXISTING CURB AND/OR GUTTER
- 3 RELOCATE EXISTING SIGN. SEE PAVEMENT MARKING AND SIGNAGE PLANS FOR PROPOSED SIGN LOCATION.
- 4 REMOVE EXISTING TREES AND/OR VEGETATION
- 5 ADJUST EXISTING STRUCTURE RIM, SEE GRADING PLANS FOR PROPOSED GRADE
- 6 CONVERT EXISTING STRUCTURE TO MANHOLE
- 7 ADJUST EXISTING UTILITY COVER TO MATCH PROPOSED GRADE. SEE GRADING PLANS FOR PROPOSED GRADE
- 8 RELOCATE EXISTING BANNER POLE. SEE SHEET CP-6.4 FOR PROPOSED LOCATION.
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DEMOLITION KEY:

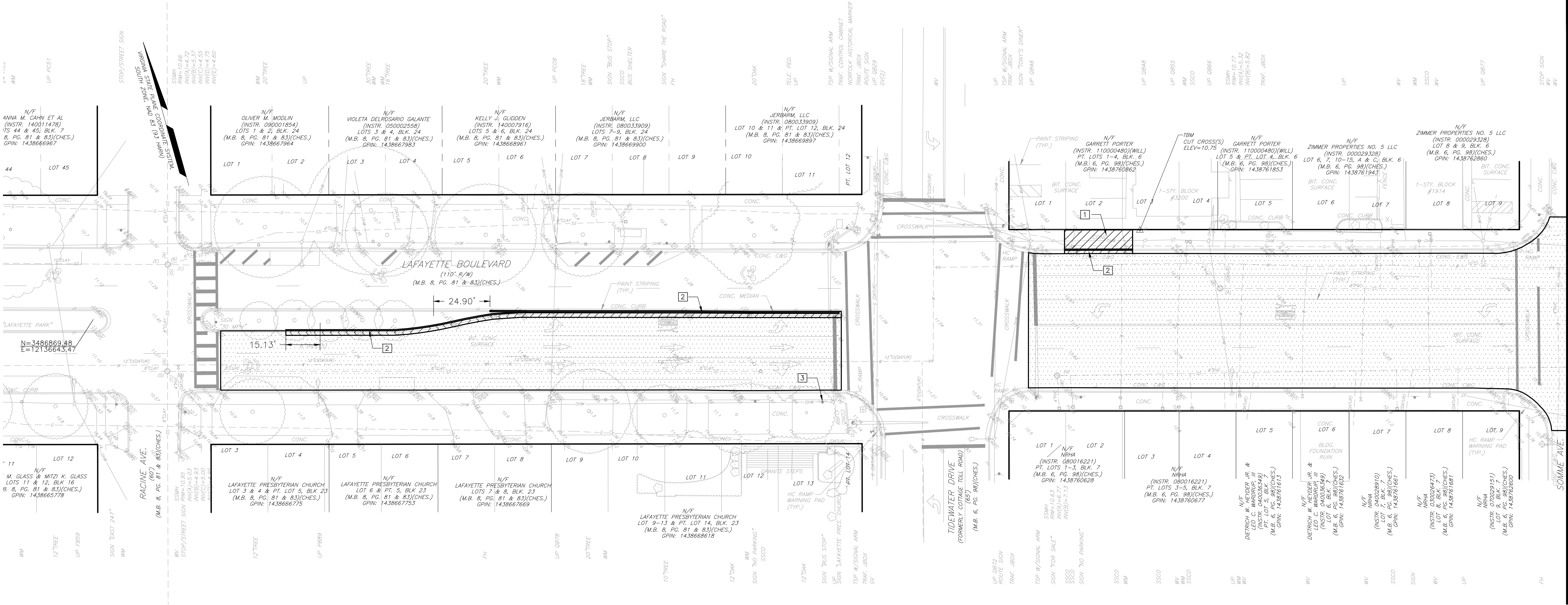
- REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT AND AGGREGATE BASE
- MILL EXISTING ASPHALT AND/OR CONCRETE PAVEMENT TO PROVIDE A MINIMUM OF 2" PAVEMENT OVERLAY. SEE GRADING PLAN FOR PROPOSED GRADES.
- REMOVE EXISTING CONCRETE SIDEWALK AND GRANITE OR CONCRETE CURB AND/OR GUTTER

DEMOLITION NOTES:

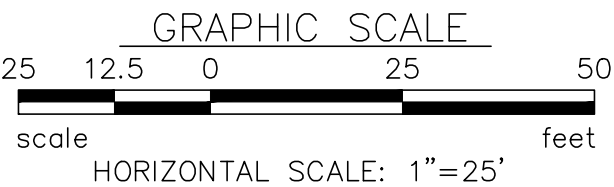
1. CONSTRUCTION SHALL BE PHASED SO THAT ONE SIDE OF THE STREET IS OPEN FOR THROUGH PEDESTRIANS AT ALL TIMES.
2. CONSTRUCTION SHALL BE COORDINATED SO THAT CUSTOMER ACCESS IS MAINTAINED TO EXISTING BUSINESSES AT ALL TIMES. THE CONTRACTOR SHALL COORDINATE WITH PROPERTY TENANTS TO PROVIDE PLYWOOD RAMPS FOR ACCESS INTO EACH AFFECTED BUSINESSES.
3. INSTALL EROSION CONTROL MEASURES SHOWN AT THE INITIAL PHASE OF DEMOLITION. THESE EROSION CONTROL MEASURES SHALL REMAIN IN PLACE THROUGHOUT SITE CONSTRUCTION AND UNTIL FINAL SITE STABILIZATION AND DENUDED AREAS ARE ESTABLISHED.
4. WHEREVER DEMOLITION WORK IS BEING PERFORMED, THE CONTRACTOR SHALL MEET ALL CODES AND REQUIREMENTS FOR PROVIDING WARNINGS AND BARRIERS TO PROTECT THE PERSONNEL. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO AVOID UNNECESSARY DAMAGE TO EXISTING SURFACES.
5. ALL EXISTING ITEMS TO REMAIN WHICH ARE DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO LIKE OR BETTER CONDITION AT THE EXPENSE OF THE CONTRACTOR.
6. CURBS AND PAVEMENT TO BE DEMOLISHED SHALL BE SAW CUT ALONG PERPENDICULAR OR RADIAL TO THE EDGES OF CONCRETE OR PAVEMENT.
7. THE CONTRACTOR SHALL COORDINATE DIRECTLY WITH DOMINION POWER (CHERYL HUNT, 393-3808 CELL 434-1753) FOR THE REMOVAL OR RELOCATION OF INDICATED POLES, GUYS AND OVERHEAD CABLE POWER FACILITIES.
8. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYORS MAKE NO GUARANTEE THE UNDERGROUND AREA, EITHER IS IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE.
9. BEFORE YOU DIG, TO MISS THE UTILITIES CALL MISS UTILITY OF VIRGINIA, TOLL FREE 811.
10. DEMOLITION DEBRIS TO BE DISPOSED OF OFF-SITE AT SITE PROCURED BY CONSTRUCTION/DEMOLITION CONTRACTOR.
11. EXISTING STORM DRAIN SYSTEM TO REMAIN INTACT AND IN A FUNCTIONAL STATE UNLESS OTHER WISE SPECIFIED.
12. ANY DISTURBED AREAS OF THE SITE ARE TO BE GRADED TO DRAIN TO EXISTING STORM INLETS, UNLESS OTHERWISE NOTED.
13. UPON COMPLETION OF DEMOLITION AND FINAL GRADING ACTIVITIES, CONTRACTOR TO EXAMINE STORM DRAIN LINES WITHIN DEMOLITION'S LIMITS AND CLEAN/ROD/JET OUT ACCUMULATED SEDIMENTS.
14. ALL PITS, EXCAVATIONS, TRENCHES CAUSED BY DEMOLITION ACTIVITIES SHALL BE BACKFILLED WITH SELECT MATERIAL AND COMPACTED PER SPECS.
15. FOR SPECIFIC DEMOLITION REQUIREMENTS REFER TO TECHNICAL SPECIFICATIONS.
16. ALL EXISTING PARKING METERS SHALL BE REMOVED, PAINTED AND RESET AT SPECIFIED LOCATIONS. DAMAGED POLES SHALL BE REPLACED.
17. ALL EXISTING SIGNS SHALL BE REMOVED AND REPLACED AT ORIGINAL LOCATIONS, UNLESS OTHERWISE SPECIFIED.
18. NORFOLK UTILITIES SANITARY SEWER MANHOLES, SANITARY SEWER CLEANOUTS, WATER MAIN VALVE BOXES, AND WATER METER BOXES SHALL BE RESET TO FINISHED GRADE. PAVING ADJUSTMENT FRAMES SHALL NOT BE USED TO ADJUST STRUCTURES TO GRADE.
19. CONTRACTOR SHALL OBTAIN A TREE PERMIT PRIOR TO REMOVAL OF EXISTING STREET TREES.
20. A CERTIFIED ARBORIST IS REQUIRED TO REMOVE OR TRIM ANY CITY OF NORFOLK TREE.

PAVEMENT MARKING ERADICATION:

1. CONTRACTOR SHALL ERADICATE ALL EXISTING PAVEMENT MARKINGS ALONG THE EAST BOUND LANES ON LAFAYETTE BOULEVARD FROM STATION 10+55 TO 13+28.
2. CONTRACTOR SHALL ERADICATE ALL EXISTING PAVEMENT MARKINGS ALONG LAFAYETTE BOULEVARD FROM STATION 13+96 TO 43+45 TO ALLOW FOR PROPOSED PAVEMENT MARKINGS SHOWN ON THE PAVEMENT MARKING AND SIGNAGE PLANS.



MATCHLINE- STATION 16+50, SEE SHEET CX-3.1 FOR CONTINUATION



CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
DEMOLITION PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

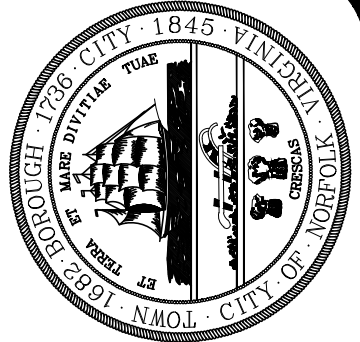
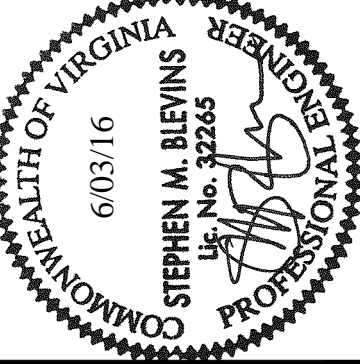
ISSUE DATE:
2/22/16

SHEET
CD-4.0

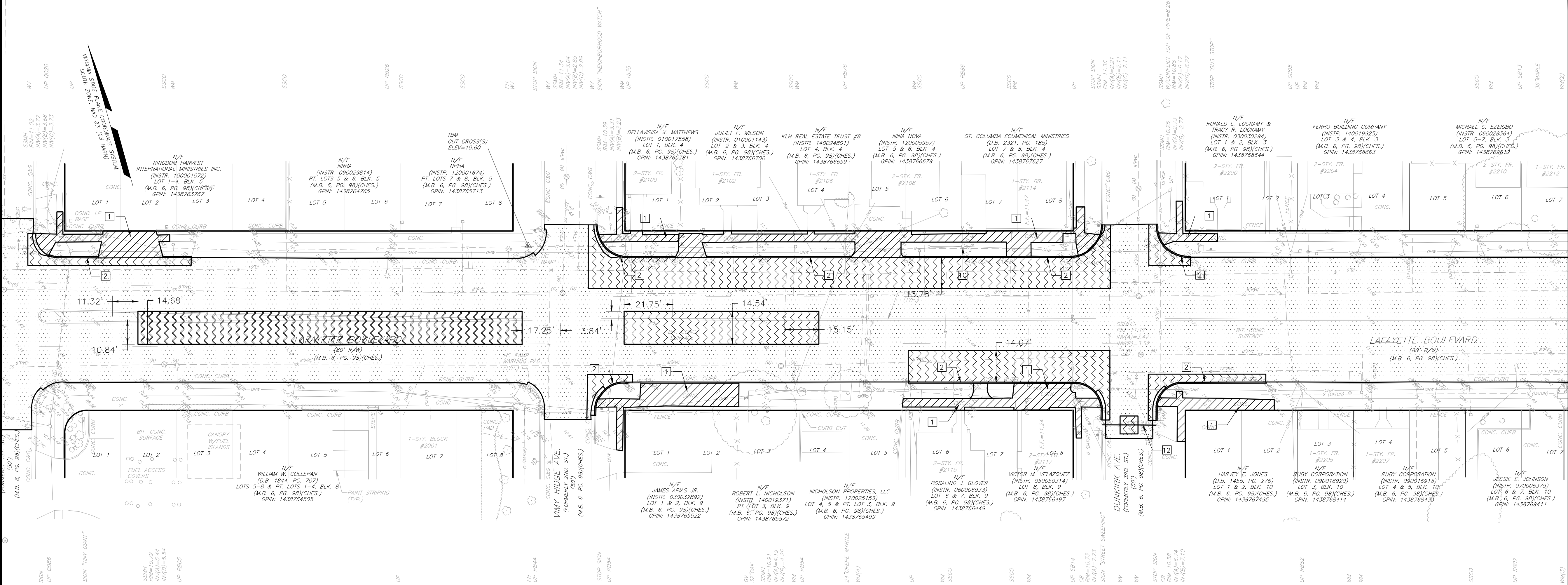


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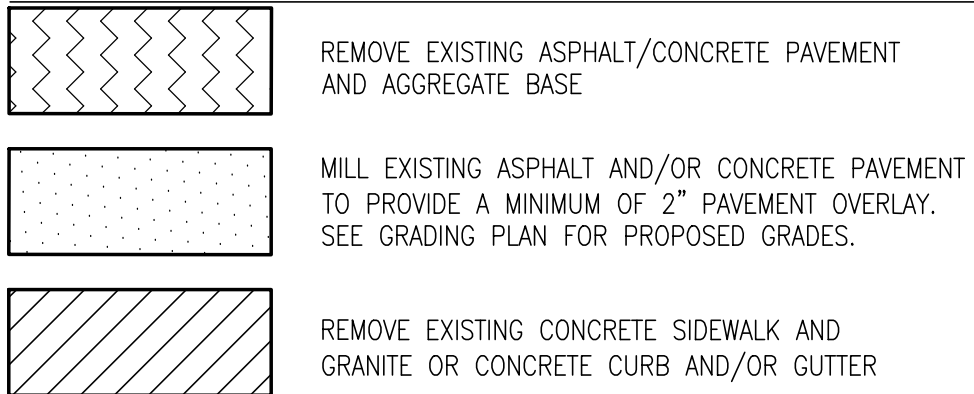
MATCHLINE- STATION 16+50. SEE SHEET CD-4.0 FOR CONTINUATION




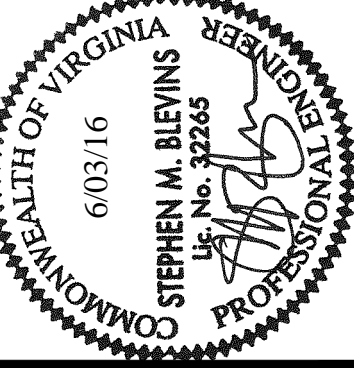
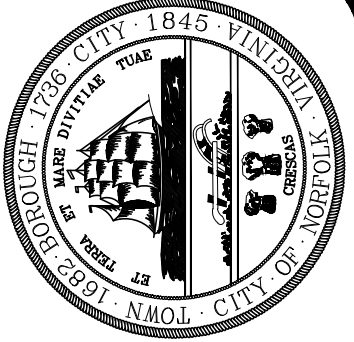
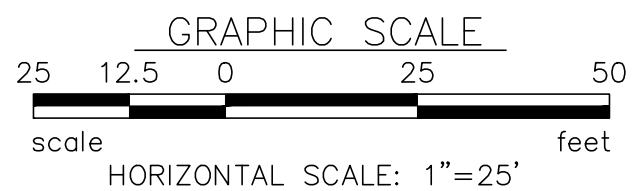
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DEMOLITION KEY:



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
DEMOLITION PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

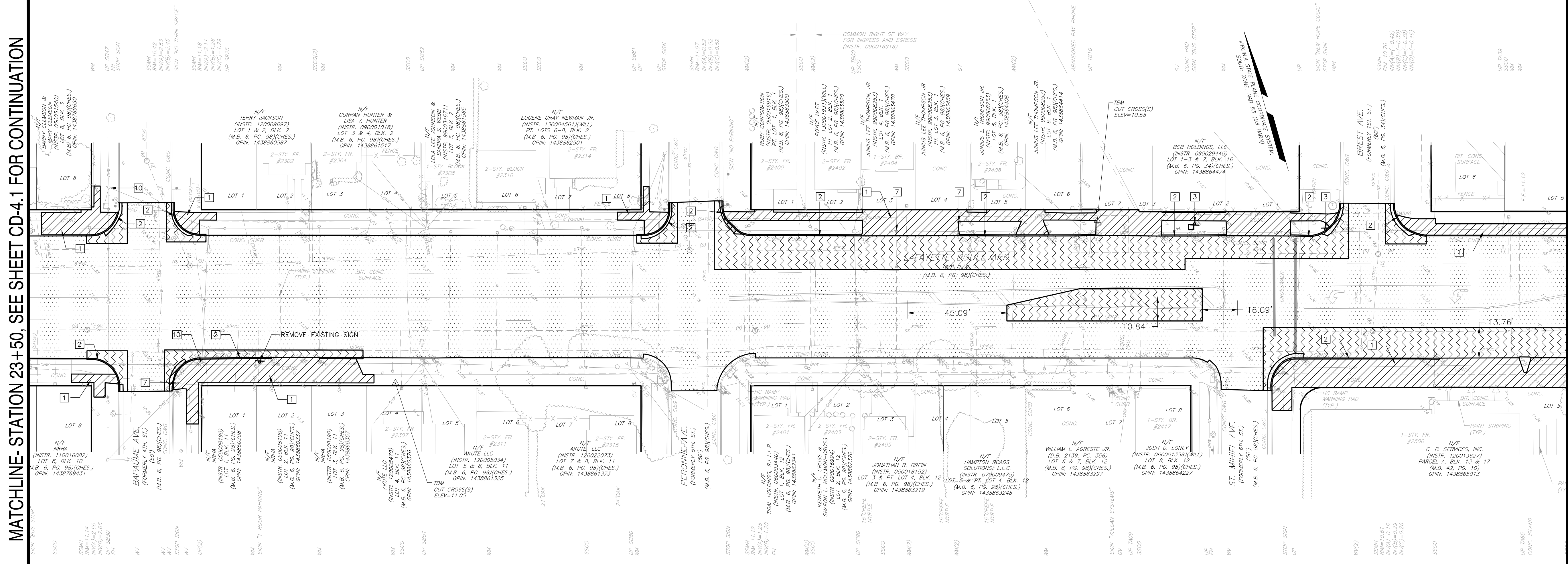
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DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CD-4.1

MATCHLINE- STATION 23+50, SEE SHEET CD-4.1 FOR CONTINUATION



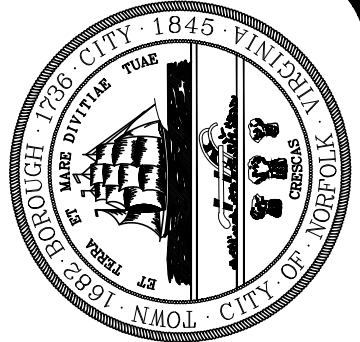
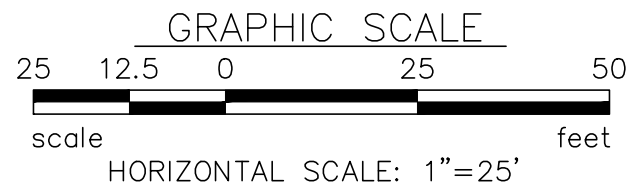
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DEMOLITION LEGEND:

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DEMOLITION KEY:

- REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT AND AGGREGATE BASE
- MILL EXISTING ASPHALT AND/OR CONCRETE PAVEMENT TO PROVIDE A MINIMUM OF 2" PAVEMENT OVERLAY. SEE GRADING PLAN FOR PROPOSED GRADES.
- REMOVE EXISTING CONCRETE SIDEWALK AND GRANITE OR CONCRETE CURB AND/OR GUTTER



CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
DEMOLITION PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

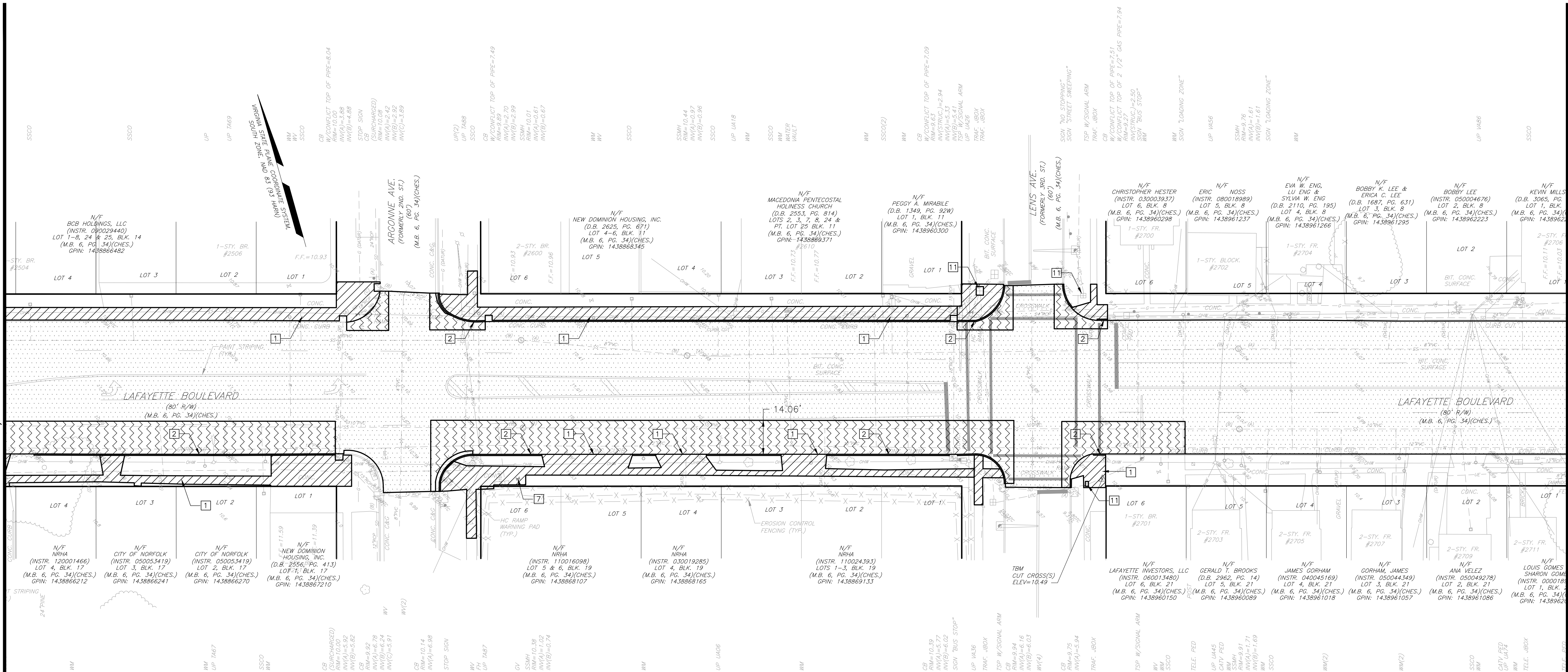
REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CD-4.2

MATCHLINE- STATION 30+50, SEE SHEET CD-4.2 FOR CONTINUATION



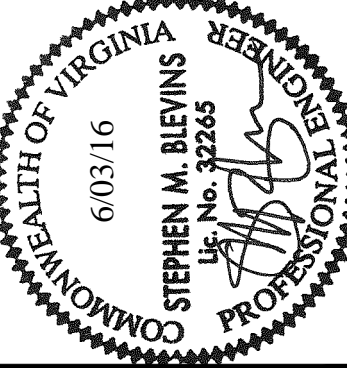
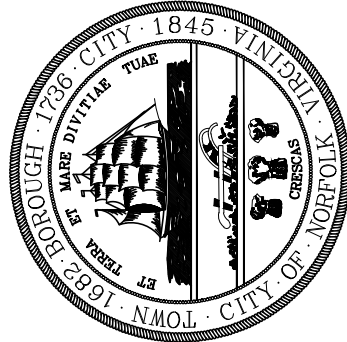
MATCHLINE- STATION 37+00, SEE SHEET CD-4.4 FOR CONTINUATION

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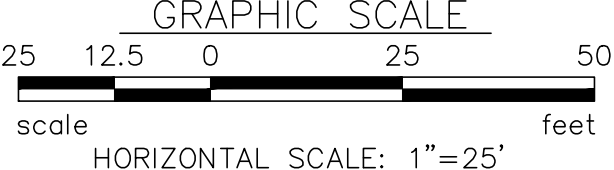
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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
DEMOLITION PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE:
2/22/16

SHEET
CD-4.3



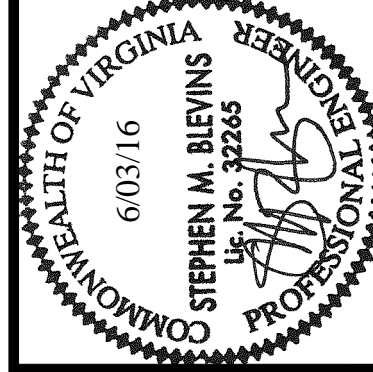
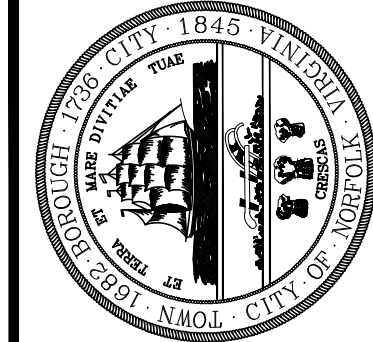
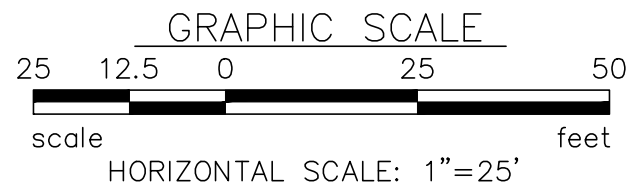
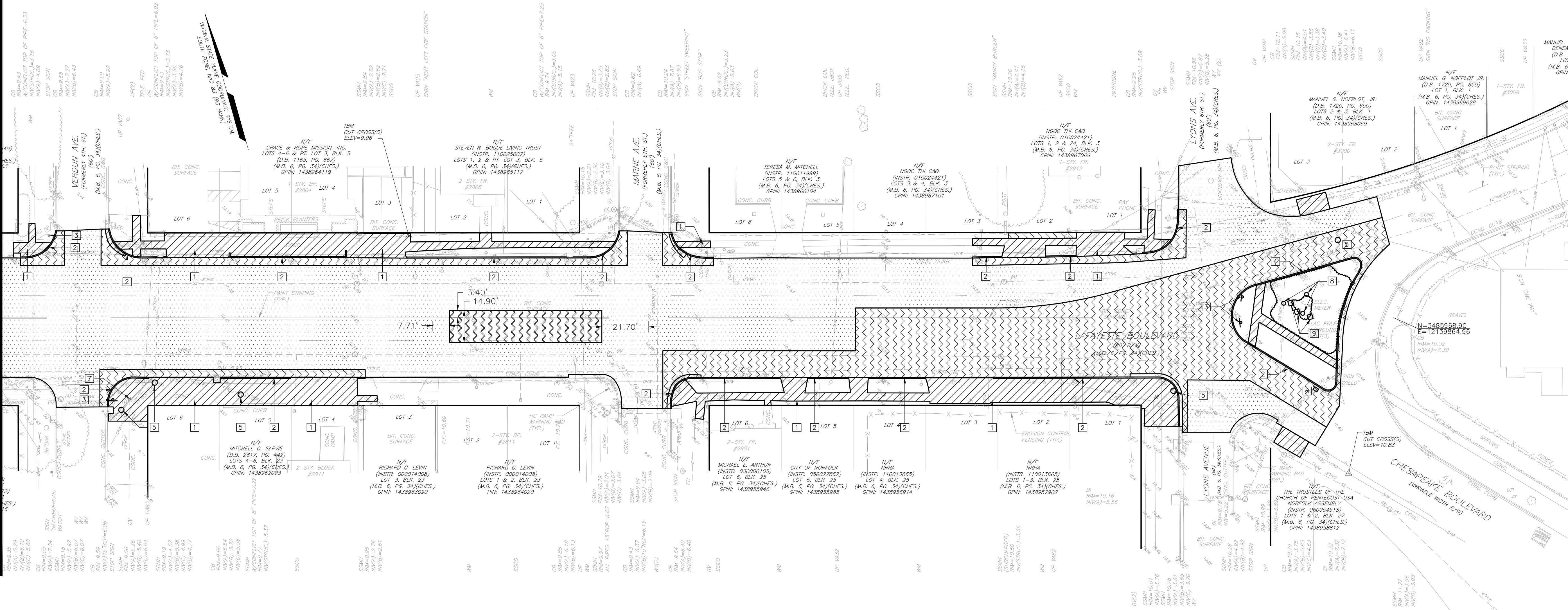
MATCHLINE- STATION 37+00, SEE SHEET CD-4.3 FOR CONTINUATION

DEMOLITION LEGEND:

- 1 SAWCUT AND REMOVE EXISTING CONCRETE DRIVEWAY OR SIDEWALK
- 2 SAWCUT AND REMOVE EXISTING CURB AND/OR GUTTER
- 3 RELOCATE EXISTING SIGN. SEE PAVEMENT MARKING AND SIGNAGE PLANS FOR PROPOSED SIGN LOCATION.
- 4 REMOVE EXISTING TREES AND/OR VEGETATION
- 5 ADJUST EXISTING STRUCTURE RIM, SEE GRADING PLANS FOR PROPOSED GRADE
- 6 CONVERT EXISTING STRUCTURE TO MANHOLE
- 7 ADJUST EXISTING UTILITY COVER TO MATCH PROPOSED GRADE. SEE GRADING PLANS FOR PROPOSED GRADE.
- 8 RELOCATE EXISTING BANNER POLE. SEE SHEET CP-6.4 FOR PROPOSED LOCATION.
- 9 REMOVE EXISTING BANNER POLE LIGHT. EXISTING ELECTRICAL METER SHALL REMAIN FOR FEATURE PUBLIC ART PROJECT.
- 10 CONTRACTOR TO COORDINATE WITH DOMINION VIRGINIA POWER DURING ALL PHASES OF THIS PROJECT. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL COORDINATE WITH DOMINION VIRGINIA POWER AND THE CITY OF NORFOLK TO DETERMINE ANY RELOCATION OF EXISTING UTILITY.
- 11 CONTRACTOR TO COORDINATE WITH CITY OF NORFOLK'S TRAFFIC ENGINEERING DEPARTMENT DURING ALL PHASES OF THIS PROJECT. CONTRACTOR TO USE CAUTION WHEN WORKING IN VICINITY OF UTILITY AND SHALL PROVIDE SUFFICIENT PROTECTION TO THE UTILITY DURING ANY NEARBY EXCAVATION.

DEMOLITION KEY:

- REMOVE EXISTING ASPHALT/CONCRETE PAVEMENT AND AGGREGATE BASE
- MILL EXISTING ASPHALT AND/OR CONCRETE PAVEMENT TO PROVIDE A MINIMUM OF 2" PAVEMENT OVERLAY. SEE GRADING PLAN FOR PROPOSED GRADES.
- REMOVE EXISTING CONCRETE SIDEWALK AND GRANITE OR CONCRETE CURB AND/OR GUTTER



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

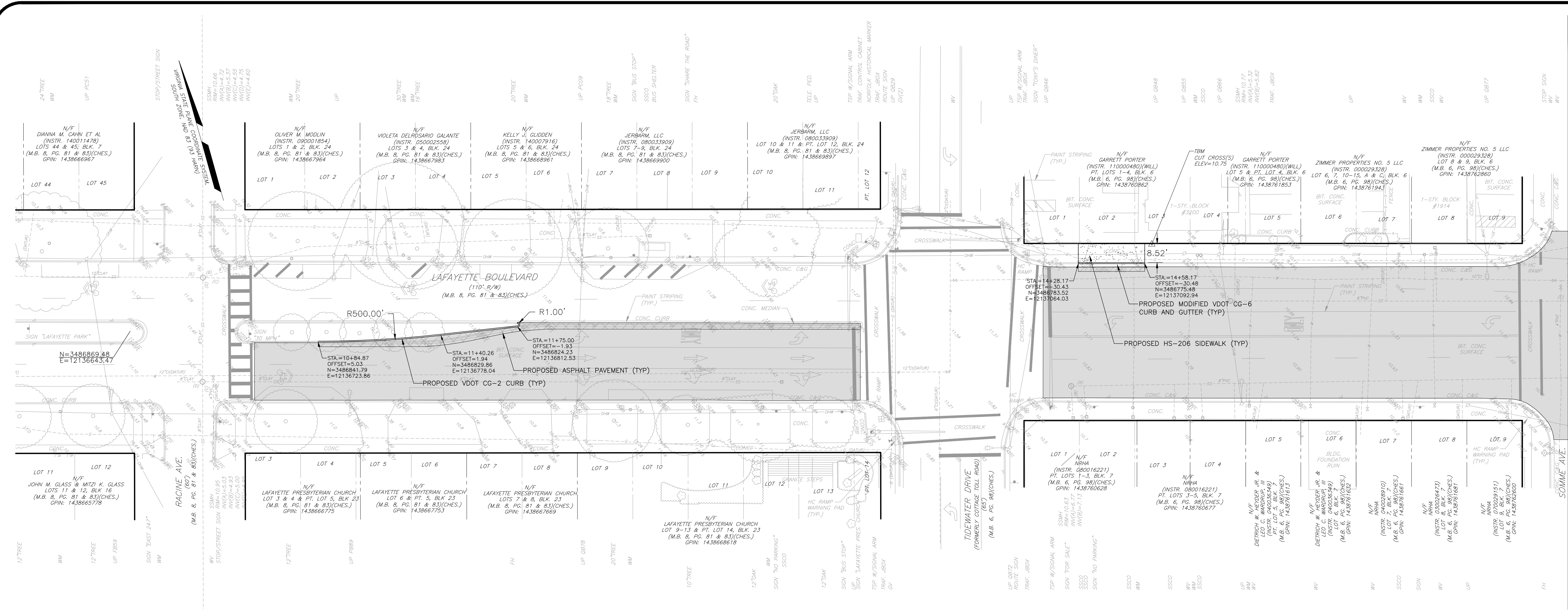
DEMOLITION PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE PREPARED BY: E. TOLLE	5/24/16

ISSUE DATE:
2/22/16

SHEET
CD-4.4



MATCHLINE- STATION 16+50, SEE SHEET CL-5.1 FOR CONTINUATION

LAYOUT GENERAL NOTES:

- CONSTRUCTION STAKEOUT – ALL CONSTRUCTION STAKEOUT SHALL BE PERFORMED BY A LICENSED SURVEYOR.
- PRIVATE UTILITY COORDINATION – CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REMOVAL/RELOCATING OF ANY PRIVATE UTILITIES WITH THE APPROPRIATE UTILITY COMPANY.
- CONSTRUCTION MATERIALS AND METHODS SHALL CONFORM TO CITY STANDARDS.
- EXCAVATED MATERIAL SHALL BE DISPOSED OF IN A LAWFUL MANNER.

CONSTRUCTION GENERAL NOTES:

- CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REMOVAL/RELOCATING OF ANY PRIVATE UTILITIES WITH THE APPROPRIATE UTILITY COMPANY.
- ALL CRACKED CONCRETE WITHIN THE RIGHT OF WAY SHALL BE REMOVED TO THE NEAREST JOINT AND REPLACED. PATCHING IS NOT ACCEPTABLE.
- ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO CITY OF NORFOLK STANDARDS.
- ALL EXCAVATED MATERIAL SHALL BE DISPOSED OF IN A LAWFUL MANNER.
- THIS PLAN DOES NOT GUARANTEE THE EXISTENCE, NON-EXISTENCE, SIZE TYPE, LOCATION, ALIGNMENT OR DEPTH OF ANY OR ALL UNDERGROUND UTILITIES OR OTHER FACILITIES, WHERE SURFACE FEATURES (MANHOLES, CATCH BASINS, VALVES, ETC.) ARE UNAVAILABLE OR INCONCLUSIVE, INFORMATION SHOWN MAY BE FROM UTILITY OWNER'S RECORDS AND/OR ELECTRONIC LINE TRACING, THE RELIABILITY OF WHICH IS UNCERTAIN. THE CONTRACTOR SHALL PERFORM WHATEVER TEST EXCAVATIONS OR OTHER INVESTIGATIONS ARE NECESSARY TO VERIFY LOCATIONS AND CLEARANCES AND SHALL REPORT IMMEDIATELY ANY DISCREPANCIES TO BLAKEWAY CORPORATION AT 757-226-0081.

RIGHT-OF-WAY NOTES:

- RIGHT-OF-WAY EXCAVATION PERMITS ARE REQUIRED TO PERFORM EXCAVATION WORK IN THE RIGHT-OF-WAY. PLEASE CONTACT THE RIGHT-OF-WAY PERMIT OFFICE AT 757-664-7306 OR AT WWW.NORFOLK.GOV/PUBLICWORKS/ROW.ASP
- WHEN A NEW APRON AND/OR SIDEWALK ARE INSTALLED, ALL NEW OR EXISTING UTILITY STRUCTURES SHALL BE INSTALLED AND/OR RELOCATED OUTSIDE THE AREA OF THE NEW APRON AND/OR SIDEWALK. A PERMIT AND INSPECTIONS ARE REQUIRED. PLEASE CONTACT THE RIGHT-OF-WAY PERMITS OFFICE AT 757-664-7306 OR AT WWW.NORFOLK.GOV/PUBLICWORKS/ROW.ASP
- ALL MISS UTILITY MARKINGS IN THE RIGHT-OF-WAY ARE REQUIRED TO BE ERADICATED AT THE COMPLETION OF THIS PROJECT IN ACCORDANCE WITH THE CITY'S ROW EXCAVATION & RESTORATION MANUAL LOCATED AT WWW.NORFOLK.GOV/PUBLICWORKS/ROW.ASP.

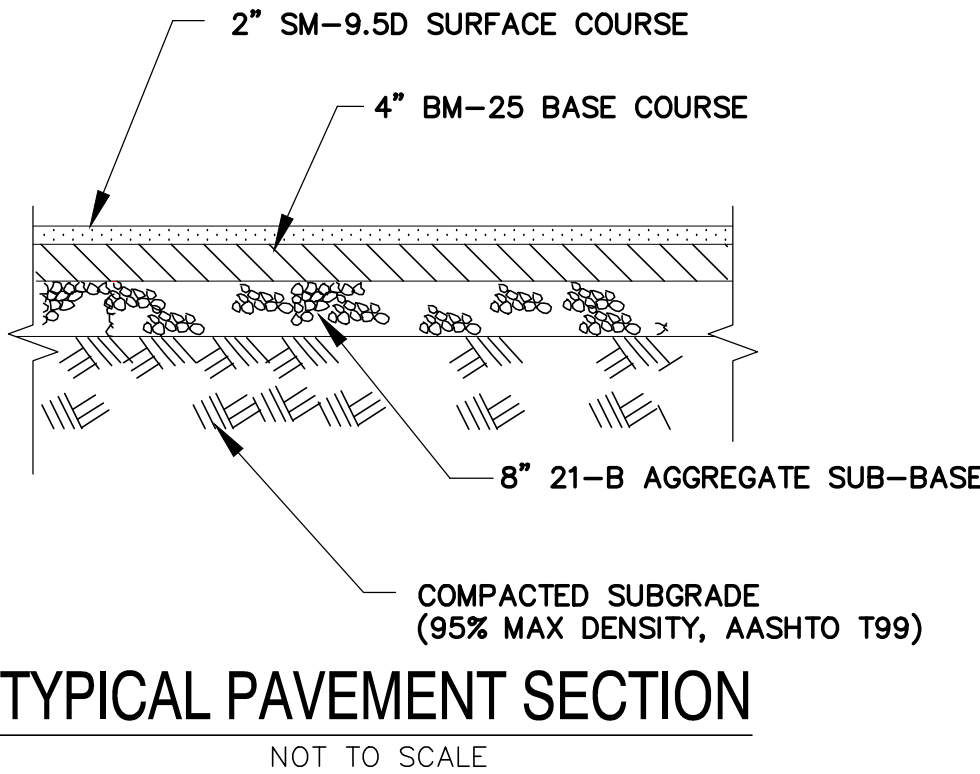
PAVEMENT NOTES:

- PAVEMENT REPLACEMENT AND DESIGN IN THE CITY RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE CITY OF NORFOLK SPECIFICATIONS AND STANDARDS.
- MATERIALS DETERMINED UNSUITABLE FOR FOUNDATIONS, SUBGRADES, OR THEIR ROADWAY PURPOSES WITHIN THE LIMITS OF CONSTRUCTION, SHALL BE EXCAVATED BY THE CONTRACTOR, AT HIS OWN EXPENSE, BELOW THE GRADE SHOWN ON PLANS AND THE AREAS SO EXCAVATED SHALL BE BACKFILLED WITH APPROVED SUITABLE MATERIALS. EXCAVATED MATERIALS SUITABLE FOR BACKFILL SHALL BE STOCKPILED SO AS NOT TO INTERFERE WITH DRAINAGE. UNSUITABLE MATERIALS SHALL BE REMOVED FROM THE SITE.
- PAVEMENT MARKING AND CROSSWALK STAMPING TO BE COMPLETED BY THE CONTRACTOR. CONTRACTOR SHALL COORDINATE WITH THE CITY OF NORFOLK TO ENSURE THAT PAVEMENT MARKINGS ARE PLACED AS SHOWN BEFORE PROJECT COMPLETION.

INCIDENTAL CONCRETE NOTES:

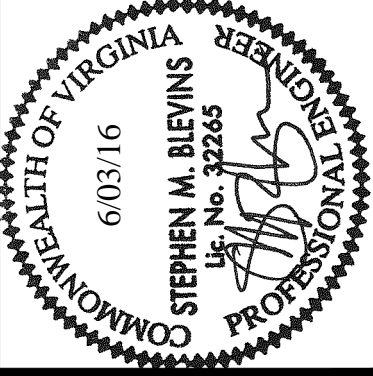
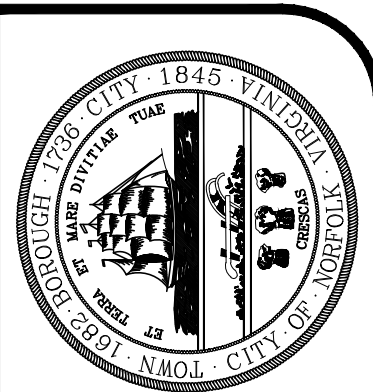
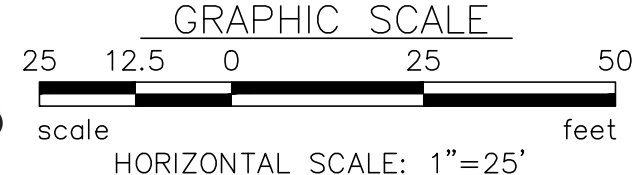
- ALL CONCRETE SHALL BE CLASS "A-3" AIR ENTRAINED (3,000 PSI) IN ACCORDANCE TO SECTION 219 OF THE 1991 VIRGINIA DEPARTMENT OF TRANSPORTATION ROADS AND BRIDGE SPECIFICATIONS UNLESS OTHERWISE SPECIFIED.
- CURB AND GUTTER SHALL BE CONSTRUCTED IN SECTIONS OF UNIFORM LENGTHS, APPROXIMATELY TEN (10') FEET (NO SECTION SHALL BE LESS THAN SIX (6) FEET). EXPANSION JOINTS SHALL BE FORMED AT INTERVALS OF ONE HUNDRED FEET (100') USING 1/2" PREMOLDED BITUMINOUS FIBER JOINT FILLER. CONSTRUCTION JOINTS ARE REQUIRED AS FOLLOWS:

CURB & GUTTER ----- 100'
SIDEWALK 6' WIDTH ----- 48'



PAVEMENT LEGEND:

- ASPHALT PAVEMENT OVERLAY (MIN OF 2")
- NEW ASPHALT PAVEMENT SECTION



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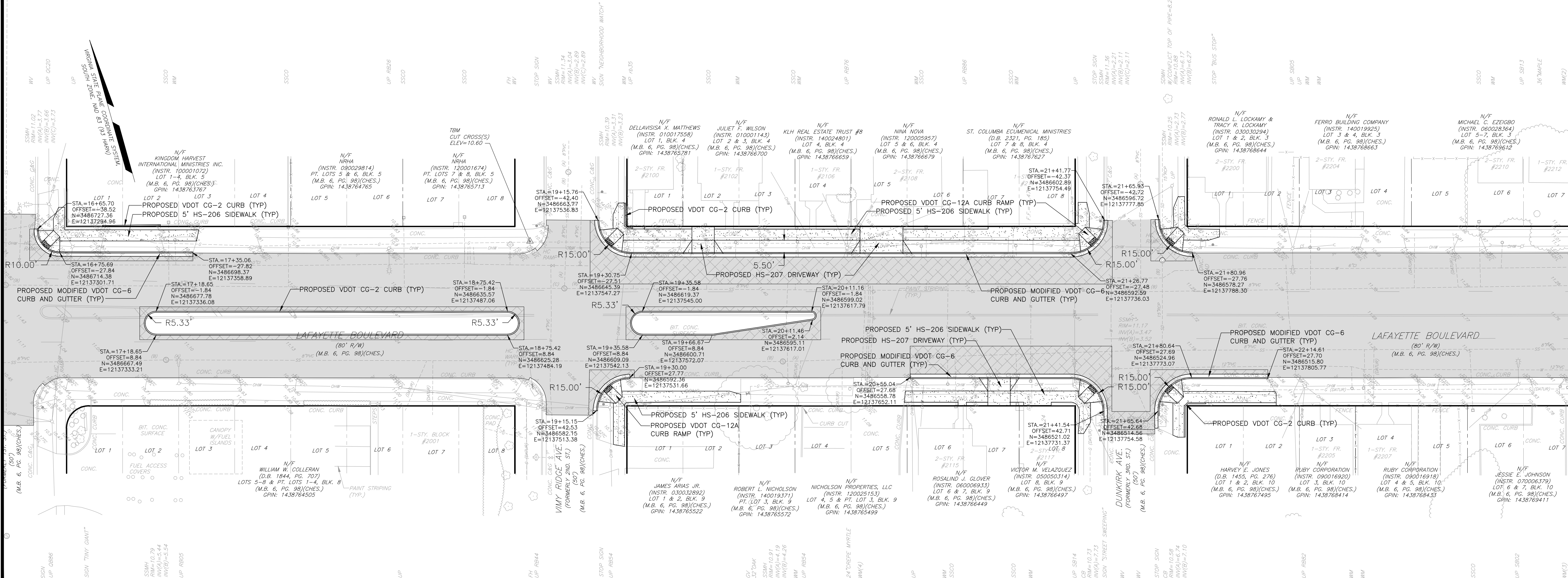
CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
LAYOUT PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

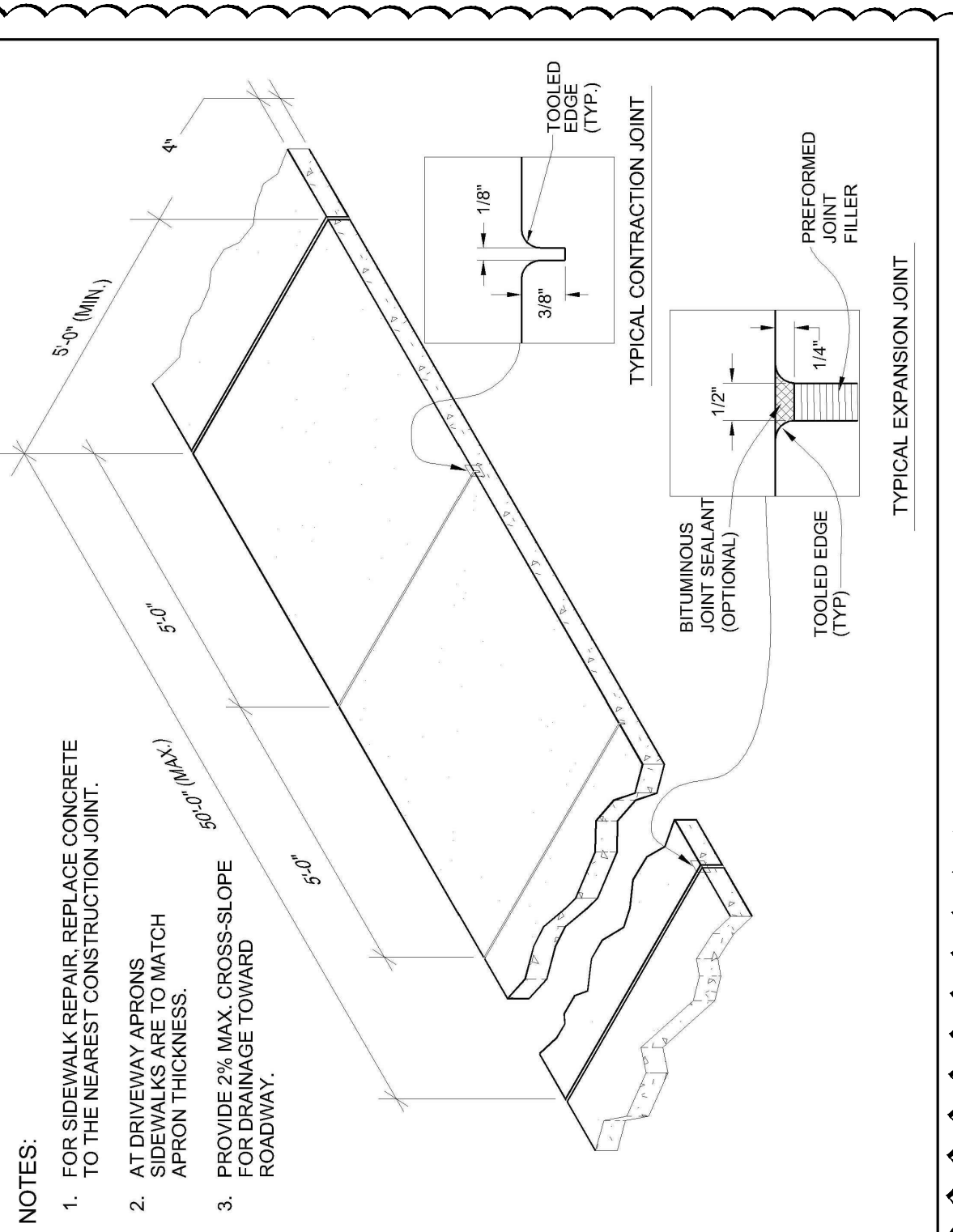
DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE:
2/22/16

SHEET
CL-5.0

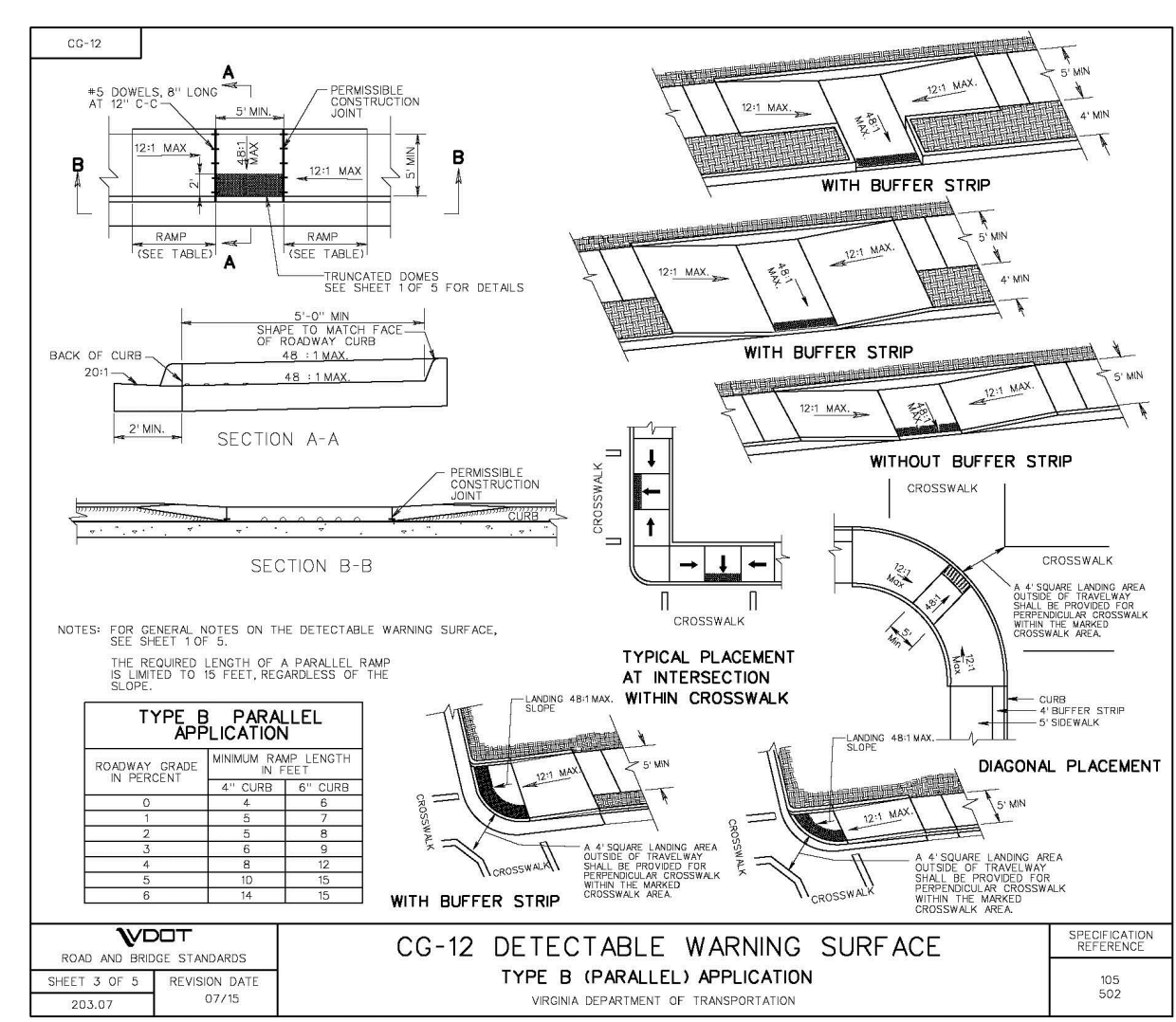
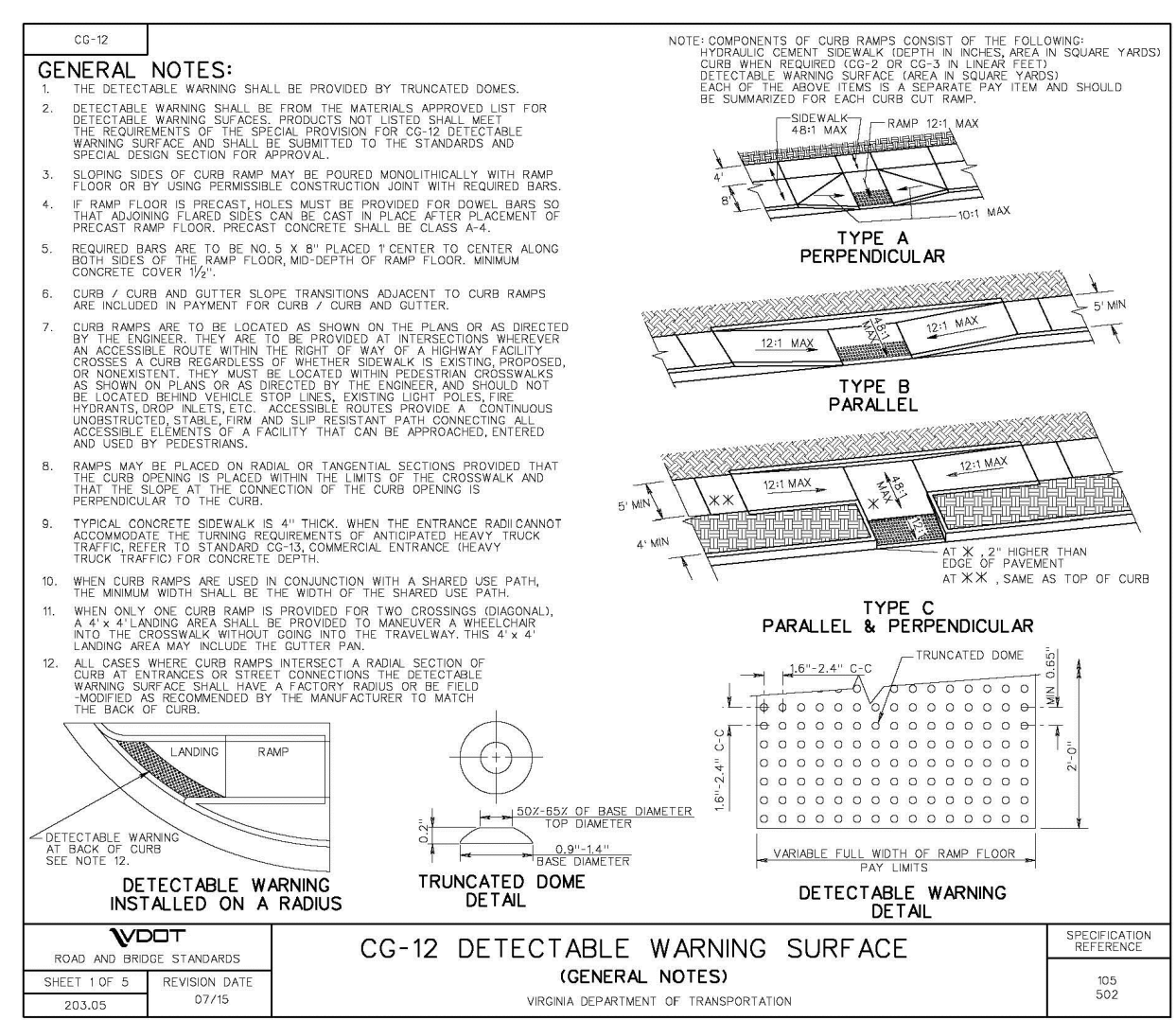
MATCHLINE- STATION 16+50, SEE SHEET CL-5.0 FOR CONTINUATION



MATCHLINE- STATION 23+50, SEE SHEET CL-5.2 FOR CONTINUATION



NOTES:	DESCRIPTION	DESIGNATION
1. FOR SIDEWALK REPAIR, REPLACE CONCRETE TO THE NEAREST CONSTRUCTION JOINT.	STANDARD SIDEWALK DETAIL	HS-206
2. AT DRIVEWAY APRONS SIDEWALKS ARE TO MATCH APRON THICKNESS.	SCALE: NOT TO SCALE	EDITION: NCDS2014.06
3. PROVIDE 2% MAX. CROSS-SLOPE FOR DRAINAGE TOWARD ROADWAY.	REVISED: JUN.2014	



PAVEMENT LEGEND:

ASPHALT PAVEMENT OVERLAY (MIN OF 2")

NEW ASPHALT PAVEMENT SECTION

va811.com
Dig With GGGG

GRAPHIC SCALE
25 12.5 0 25 50
scale HORIZONTAL SCALE: 1"=25' feet

CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

6/03/16
STEPHEN M. BLEVINS
Professional Engineer

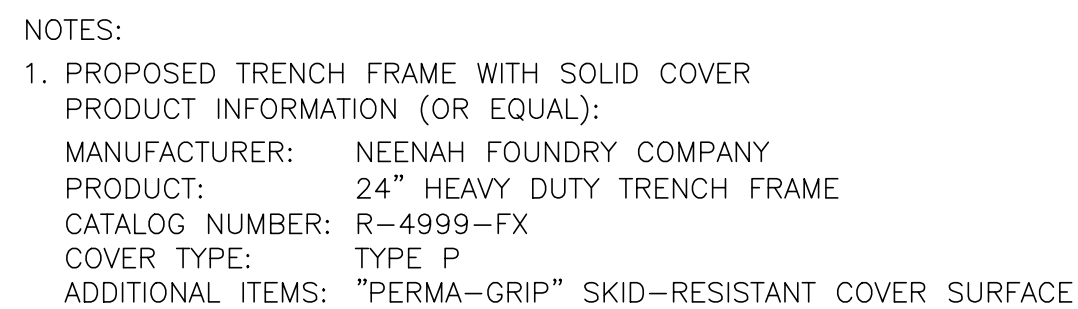
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REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CL-5.1



CONCRETE:
5% AIR-ENTRAINED;
3500 PSI (MIN.)

4" 2" RAD. 6" 1:1 SLOPE 7" 6" CRUSHED STONE

12" 6" 1'-6"

CONCRETE:
5%-7% AIR-ENTRAINED;
3500 PSI (MIN.)

4" CRUSHED STONE

CRUSHED STONE

ROADWAY SURFACE

SLOPE

18"

4"



2"

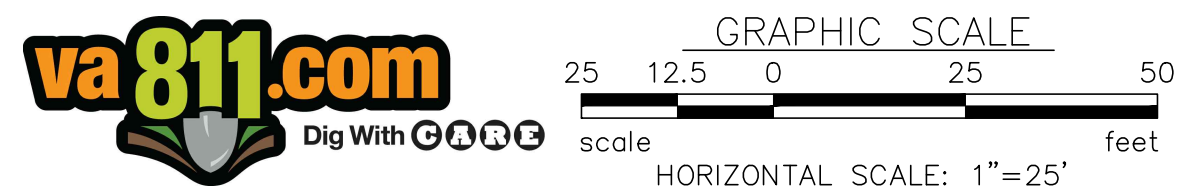
6"

12"

6"

PAVEMENT LEGEND:

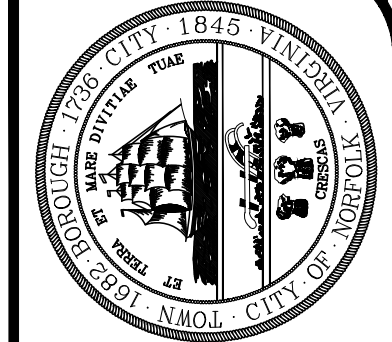
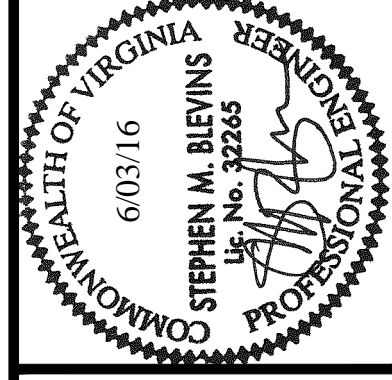
	ASPHALT PAVEMENT OVERLAY (MIN OF 2")
	NEW ASPHALT PAVEMENT SECTION

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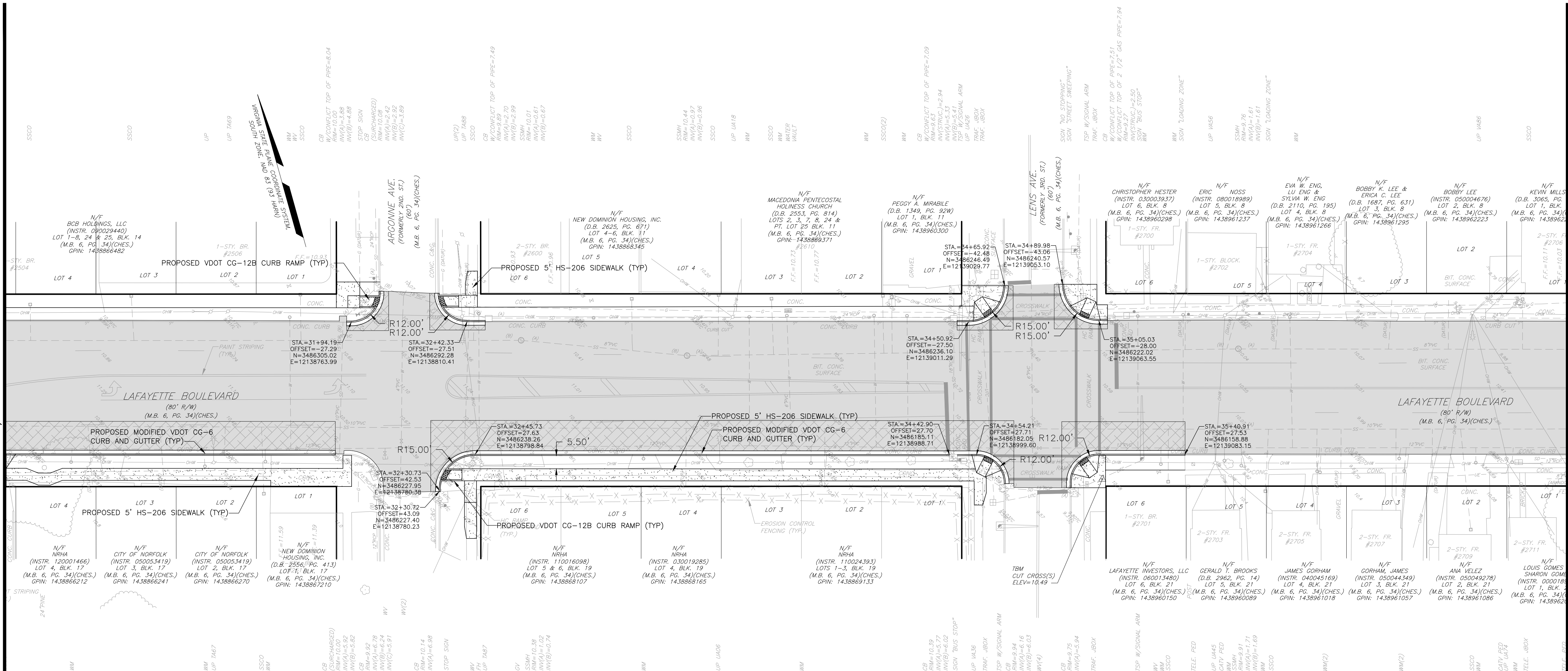
DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CL-5.2



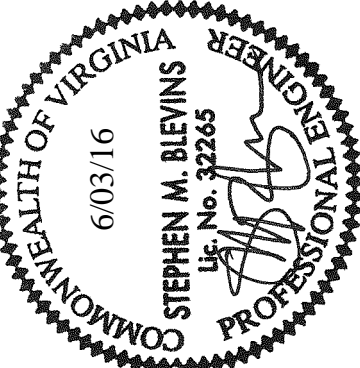
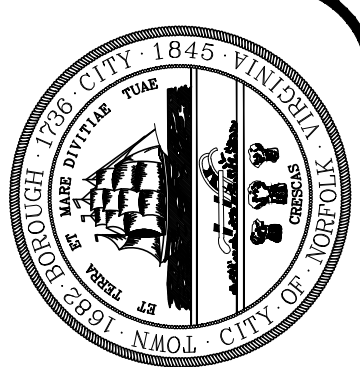
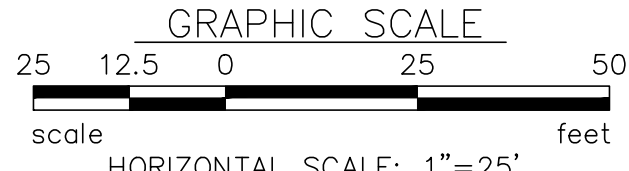
MATCHLINE- STATION 30+50, SEE SHEET CL-5.2 FOR CONTINUATION



MATCHLINE- STATION 37+00, SEE SHEET CL-5.4 FOR CONTINUATION

PAVEMENT LEGEND:

- ASPHALT PAVEMENT OVERLAY (MIN OF 2")
- NEW ASPHALT PAVEMENT SECTION



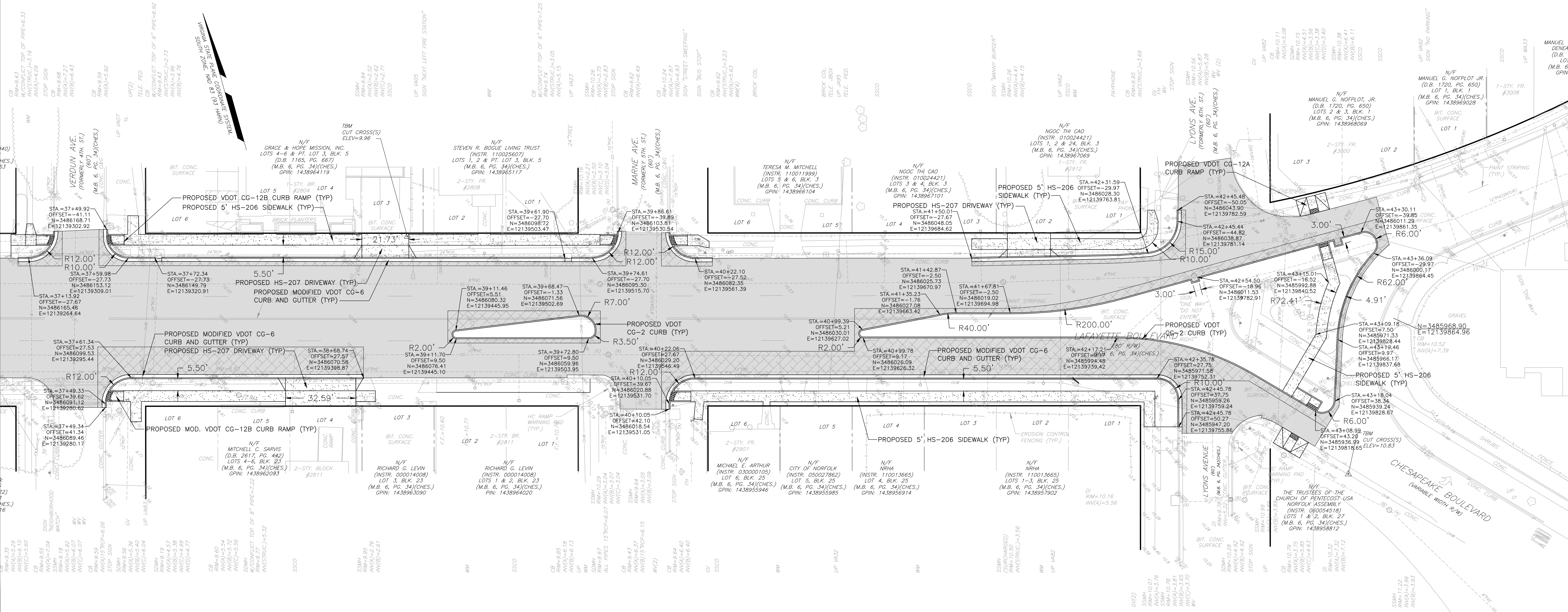
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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
LAYOUT PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

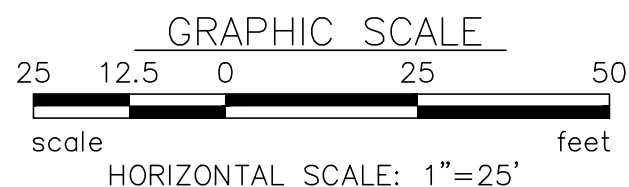
DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE:
2/22/16
SHEET
CL-5.3

MATCHLINE- STATION 37+00, SEE SHEET CL-5.3 FOR CONTINUATION



PAVEMENT LEGEND:

- ASPHALT PAVEMENT OVERLAY (MIN OF 2")
- NEW ASPHALT PAVEMENT SECTION



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 DEPARTMENT OF PUBLIC WORKS

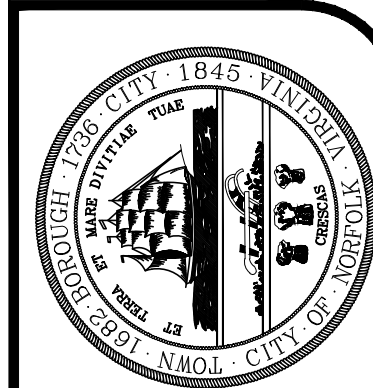
LAYOUT PLAN

LAFAYETTE BOULEVARD
 NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE	5/24/16
2	PREPARED BY: E. TOLLE	
3	ISSUE DATE:	
4	2/22/16	
5	SHEET	
6	CL-5.4	

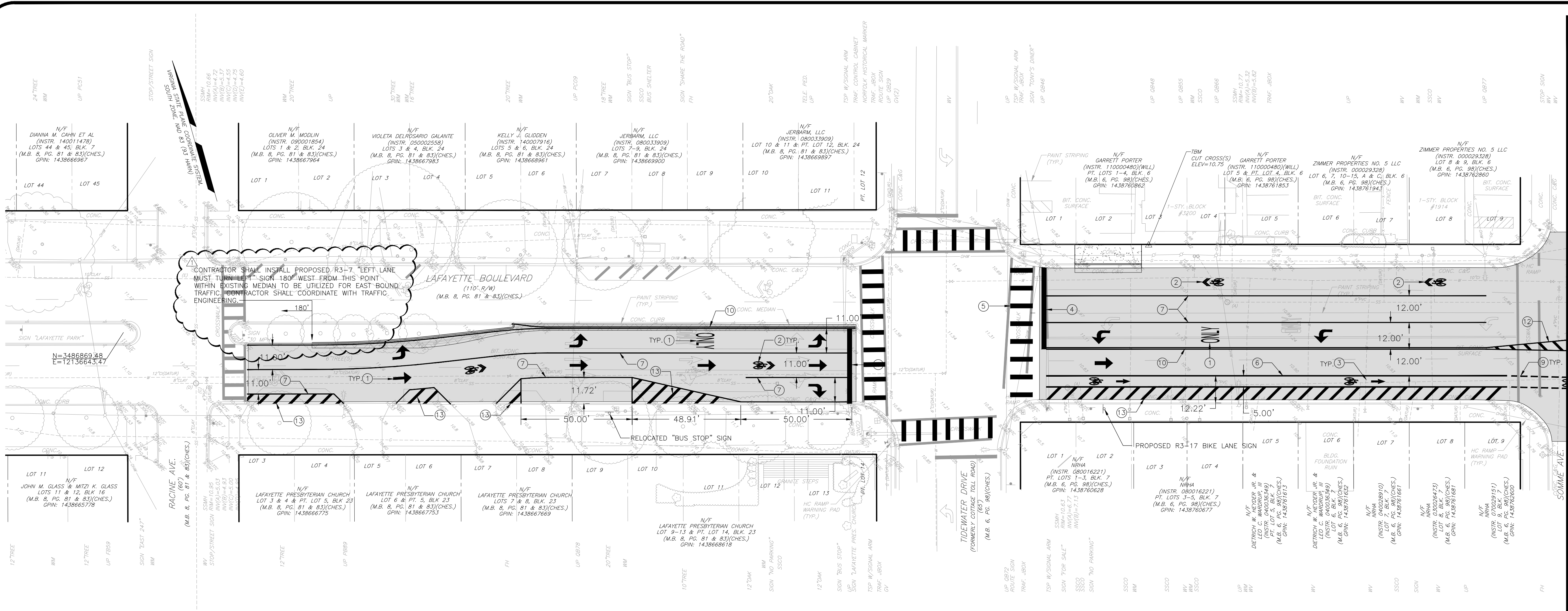
COMMONWEALTH OF VIRGINIA
 6/03/16
 ENGINEER
 STEPHEN M. BLEVINS
 License No. 22265
 PROFESSIONAL

MANUEL DENEA (D.B. 101)
 (M.B. 6, PG. 34)
 GPN: 1438963028



PAVEMENT MARKING LEGEND:

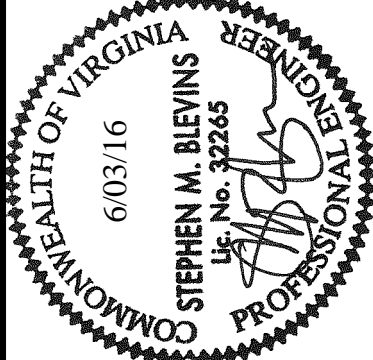
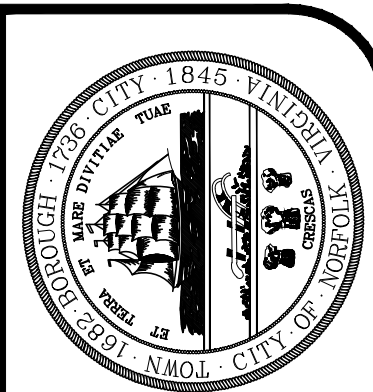
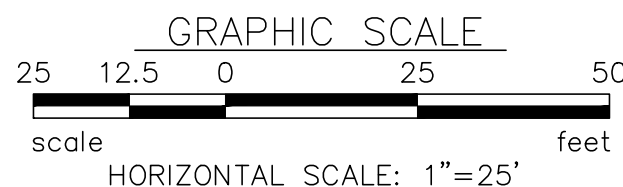
- 1 PAVEMENT LEGEND, COLOR: WHITE
- 2 "SHARED" BICYCLE LANE LEGEND, COLOR: WHITE
- 3 "DESIGNATED" BICYCLE LANE LEGEND, COLOR: WHITE
- 4 24" STOP BAR, COLOR: WHITE
- 5 CROSSWALK, COLOR: WHITE
- 6 6" SOLID LINE, COLOR: WHITE
- 7 4" SOLID LINE, COLOR: WHITE
- 8 4" SOLID LINE, COLOR: YELLOW
- 9 4" MINI SKIP LINE, COLOR: WHITE
- 10 TWO WAY BARRIER, COLOR: YELLOW
- 11 ONE WAY BARRIER, COLOR: YELLOW
- 12 CHEVRON, COLOR: YELLOW
- 13 CHEVRON, COLOR: WHITE
- 14 DOUBLE CHEVRON, COLOR: WHITE



MATCHLINE- STATION 16+50, SEE SHEET CP-6.1 FOR CONTINUATION



TYPICAL CROSS-SECTION
NOT TO SCALE



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
PAVEMENT MARKING PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16
2	CITY ADDED CROSSWALK BARS AT TIDEWATER DR	6/03/16

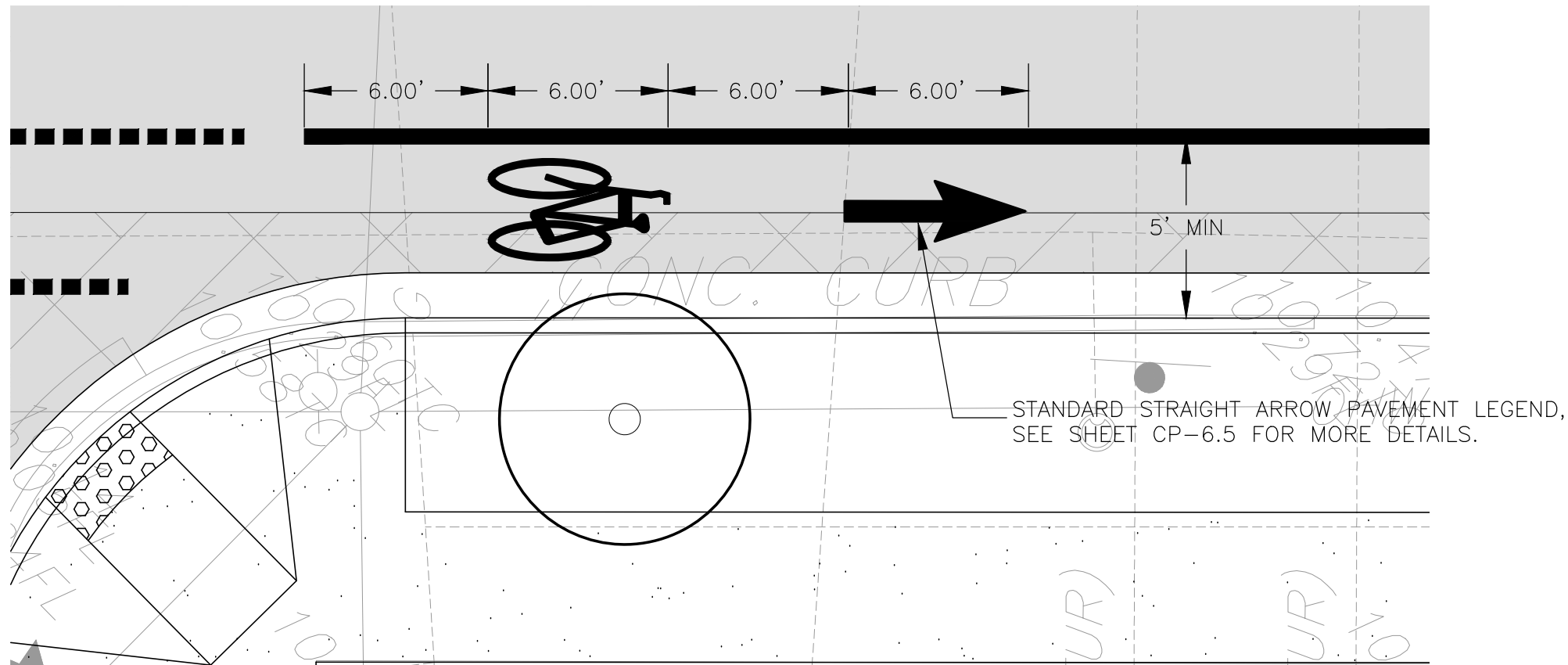
DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE:
2/22/16

SHEET
CP-6.0

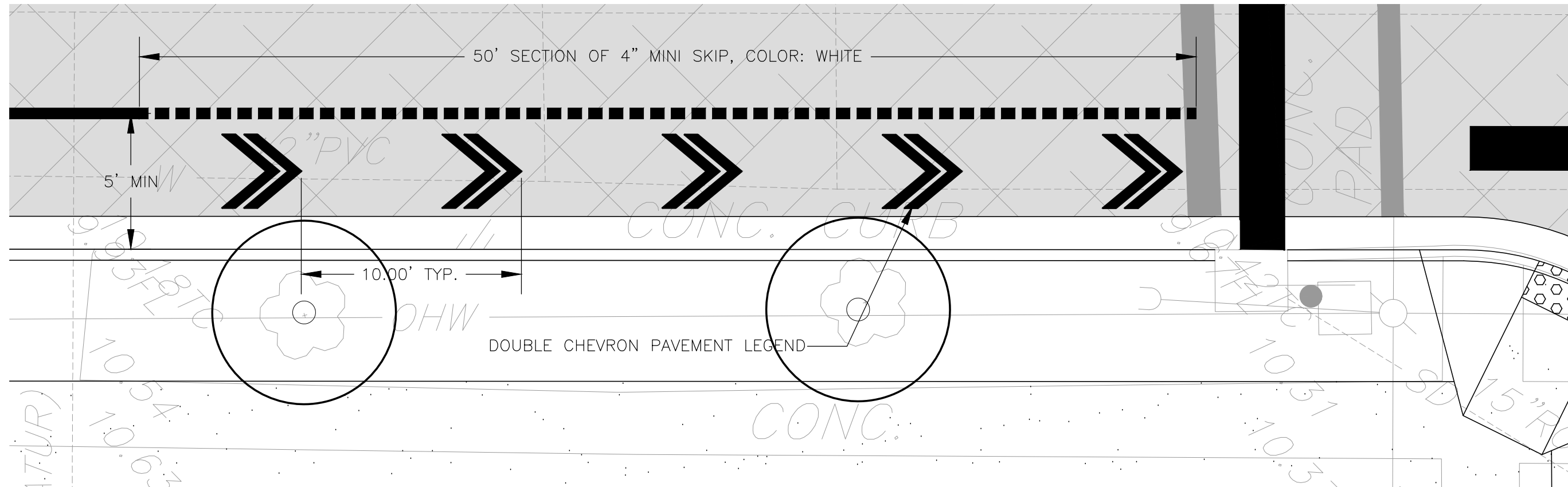
MATCHLINE- STATION 16+50, SEE SHEET CP-6.0 FOR CONTINUATION

PAVEMENT MARKING LEGEND:

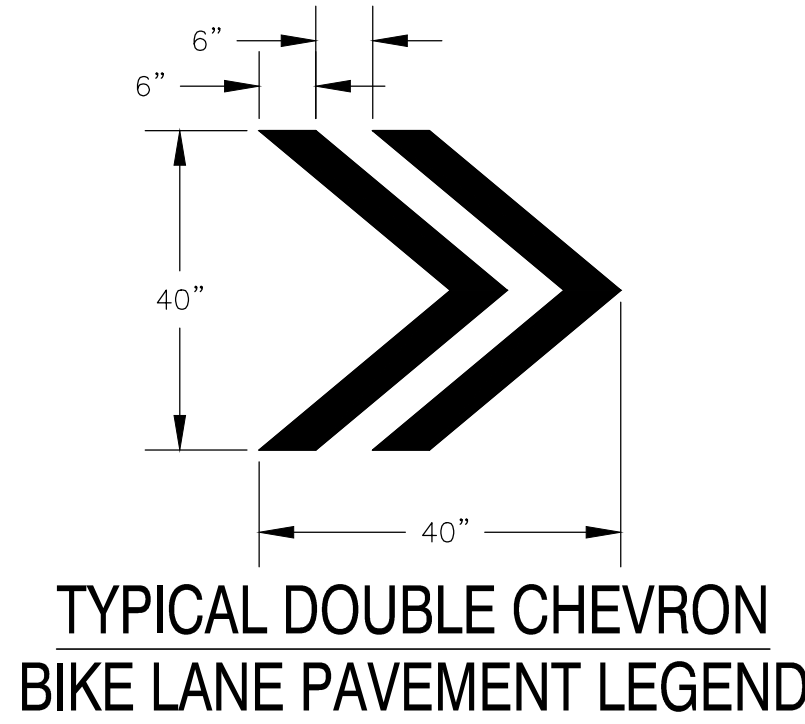
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- 2 "SHARED" BICYCLE LANE LEGEND, COLOR: WHITE
- 3 "DESIGNATED" BICYCLE LANE LEGEND, COLOR: WHITE
- 4 24" STOP BAR, COLOR: WHITE
- 5 CROSSWALK, COLOR: WHITE
- 6 6" SOLID LINE, COLOR: WHITE
- 7 4" SOLID LINE, COLOR: WHITE
- 8 4" SOLID LINE, COLOR: YELLOW
- 9 4" MINI SKIP LINE, COLOR: WHITE
- 10 TWO WAY BARRIER, COLOR: YELLOW
- 11 ONE WAY BARRIER, COLOR: YELLOW
- 12 CHEVRON, COLOR: YELLOW
- 13 CHEVRON, COLOR: WHITE
- 14 DOUBLE CHEVRON, COLOR: WHITE



TYPICAL "DESIGNATED" BICYCLE LANE LEGEND DETAIL
NOT TO SCALE



TYPICAL BICYCLE LANE PAVEMENT MARKING AT BUS STOP DETAIL
NOT TO SCALE



TYPICAL DOUBLE CHEVRON
BIKE LANE PAVEMENT LEGEND
NOT TO SCALE

MATCHLINE- STATION 23+50, SEE SHEET CP-6.2 FOR CONTINUATION

CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
PAVEMENT MARKING PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

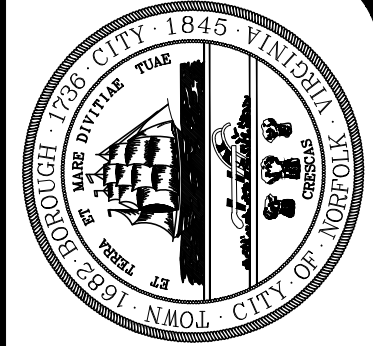
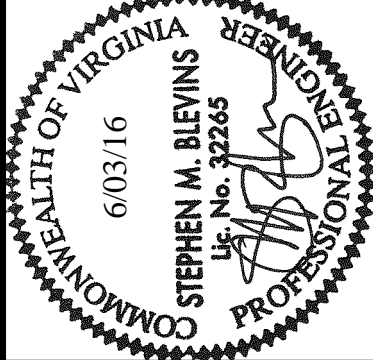
DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CP-6.1

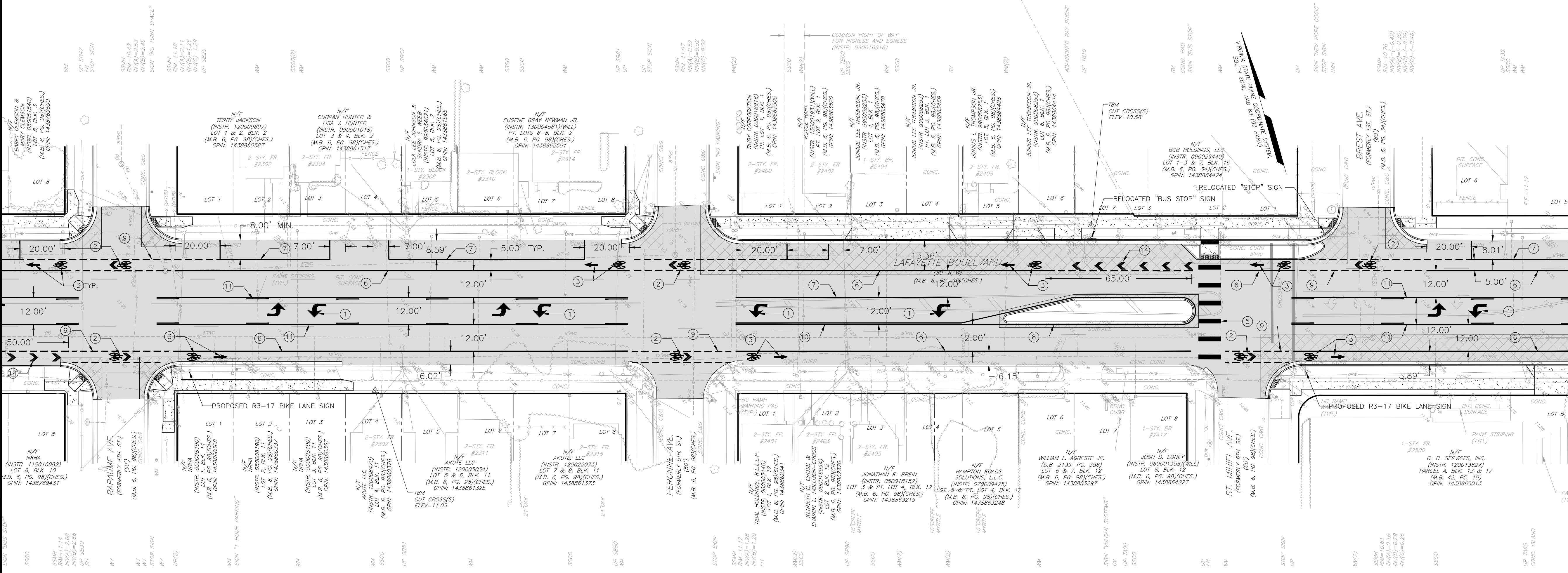


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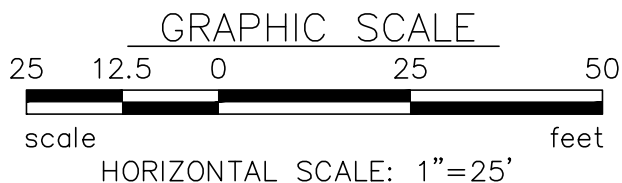


MATCHLINE- STATION 23+50, SEE SHEET CP-6.1 FOR CONTINUATION

- PAVEMENT MARKING LEGEND:
- 1 PAVEMENT LEGEND, COLOR: WHITE
 - 2 "SHARED" BICYCLE LANE LEGEND, COLOR: WHITE
 - 3 "DESIGNATED" BICYCLE LANE LEGEND, COLOR: WHITE
 - 4 24" STOP BAR, COLOR: WHITE
 - 5 CROSSWALK, COLOR: WHITE
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 - 12 CHEVRON, COLOR: YELLOW
 - 13 CHEVRON, COLOR: WHITE
 - 14 DOUBLE CHEVRON, COLOR: WHITE



MATCHLINE- STATION 30+50, SEE SHEET CP-6.3 FOR CONTINUATION



CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

PAVEMENT MARKING PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
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DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

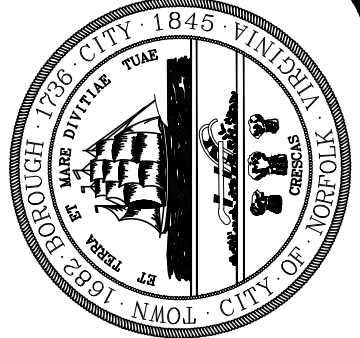
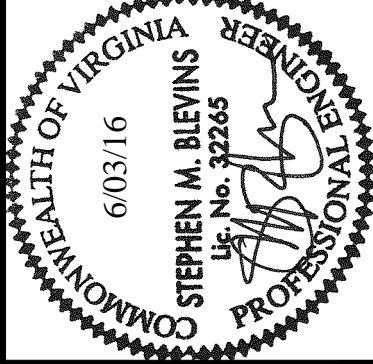
ISSUE DATE:
2/22/16

SHEET
CP-6.2

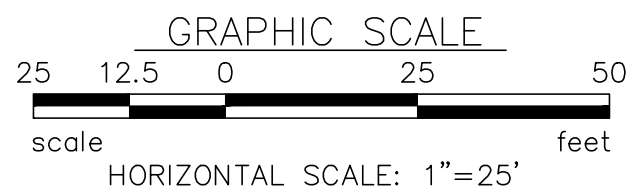


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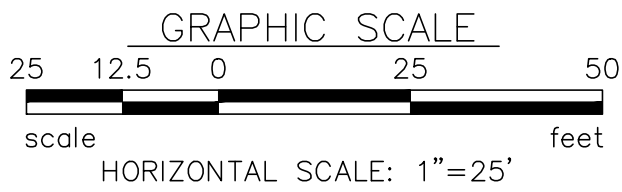
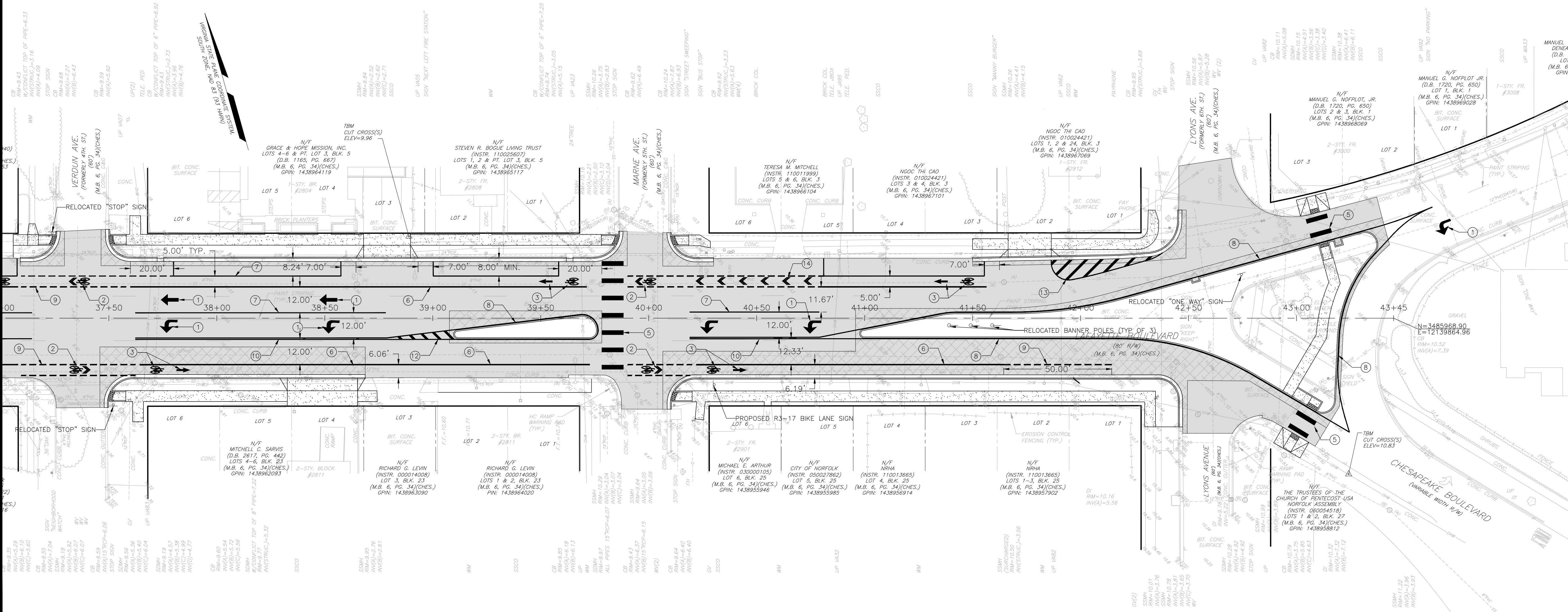



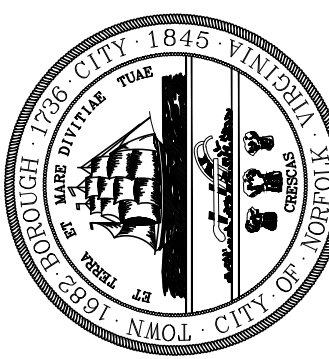
- ① PAVEMENT LEGEND, COLOR: WHITE
- ② "SHARED" BICYCLE LANE LEGEND, COLOR: WHITE
- ③ "DESIGNATED" BICYCLE LANE LEGEND, COLOR: WHITE
- ④ 24" STOP BAR, COLOR: WHITE
- ⑤ CROSOWALK, COLOR: WHITE
- ⑥ 6" SOLID LINE, COLOR: WHITE
- ⑦ 4" SOLID LINE, COLOR: WHITE
- ⑧ 4" SOLID LINE, COLOR: YELLOW
- ⑨ 4" MINI SKIP LINE, COLOR: WHITE
- ⑩ TWO WAY BARRIER, COLOR: YELLOW
- ⑪ ONE WAY BARRIER, COLOR: YELLOW
- ⑫ CHEVRON, COLOR: YELLOW
- ⑬ CHEVRON, COLOR: WHITE
- ⑭ DOUBLE CHEVRON, COLOR: WHITE



MATCHLINE- STATION 37+00, SEE SHEET CP-6.3 FOR CONTINUATION

- PAVEMENT MARKING LEGEND:
- 1 PAVEMENT LEGEND, COLOR: WHITE
 - 2 "SHARED" BICYCLE LANE LEGEND, COLOR: WHITE
 - 3 "DESIGNATED" BICYCLE LANE LEGEND, COLOR: WHITE
 - 4 24" STOP BAR, COLOR: WHITE
 - 5 CROSSWALK, COLOR: WHITE
 - 6 6" SOLID LINE, COLOR: WHITE
 - 7 4" SOLID LINE, COLOR: WHITE
 - 8 4" SOLID LINE, COLOR: YELLOW
 - 9 4" MINI SKIP LINE, COLOR: WHITE
 - 10 TWO WAY BARRIER, COLOR: YELLOW
 - 11 ONE WAY BARRIER, COLOR: YELLOW
 - 12 CHEVRON, COLOR: YELLOW
 - 13 CHEVRON, COLOR: WHITE
 - 14 DOUBLE CHEVRON, COLOR: WHITE





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DEPARTMENT OF PUBLIC WORKS

PAVEMENT MARKING PLAN

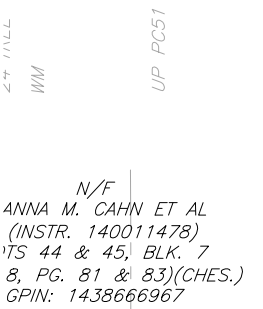
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

SHEET
CP-6.4



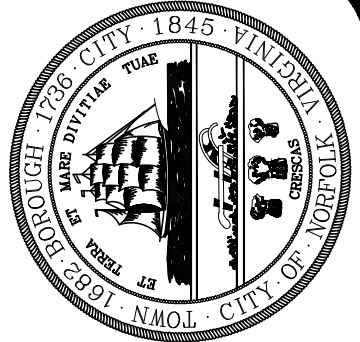
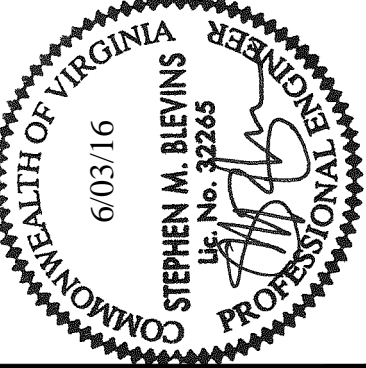
TC	= TOP OF CURB
FL	= FLOW LINE OF GUTTER
P	= ASPHALT OR CONCRETE PAVEMENT SURFACE
SW	= SIDEWALK SURFACE
G	= GROUND SURFACE
M.G.	= MATCH EXISTING GRADE
H.P.	= HIGH POINT OF GRADED SURFACE

1. ALL CROSS AND LONGITUDINAL SLOPES ALONG ALL ACCESSIBLE ROUTES SHALL BE INSTALLED IN ACCORDANCE WITH ICC/ANSI A117.1, 2003 EDITION
2. ALL STORM STRUCTURES SHALL MEET CITY STANDARDS. ALL STRUCTURES WITHIN THE RIGHT-OF-WAY SHALL MEET VDOT STANDARDS. ALL PIPE LENGTHS ARE FROM CENTER OF STRUCTURE.
3. ALL CONCRETE PIPE WITHIN THE RIGHT-OF-WAY OR SUBJECT TO TRAFFIC MUST BE CLASS III.

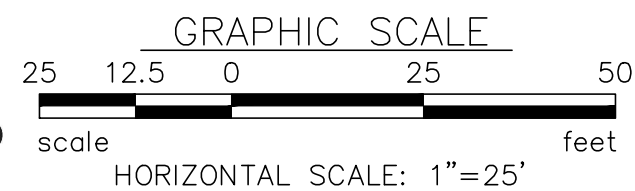


GRADING PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

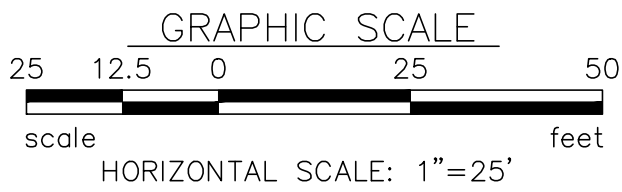
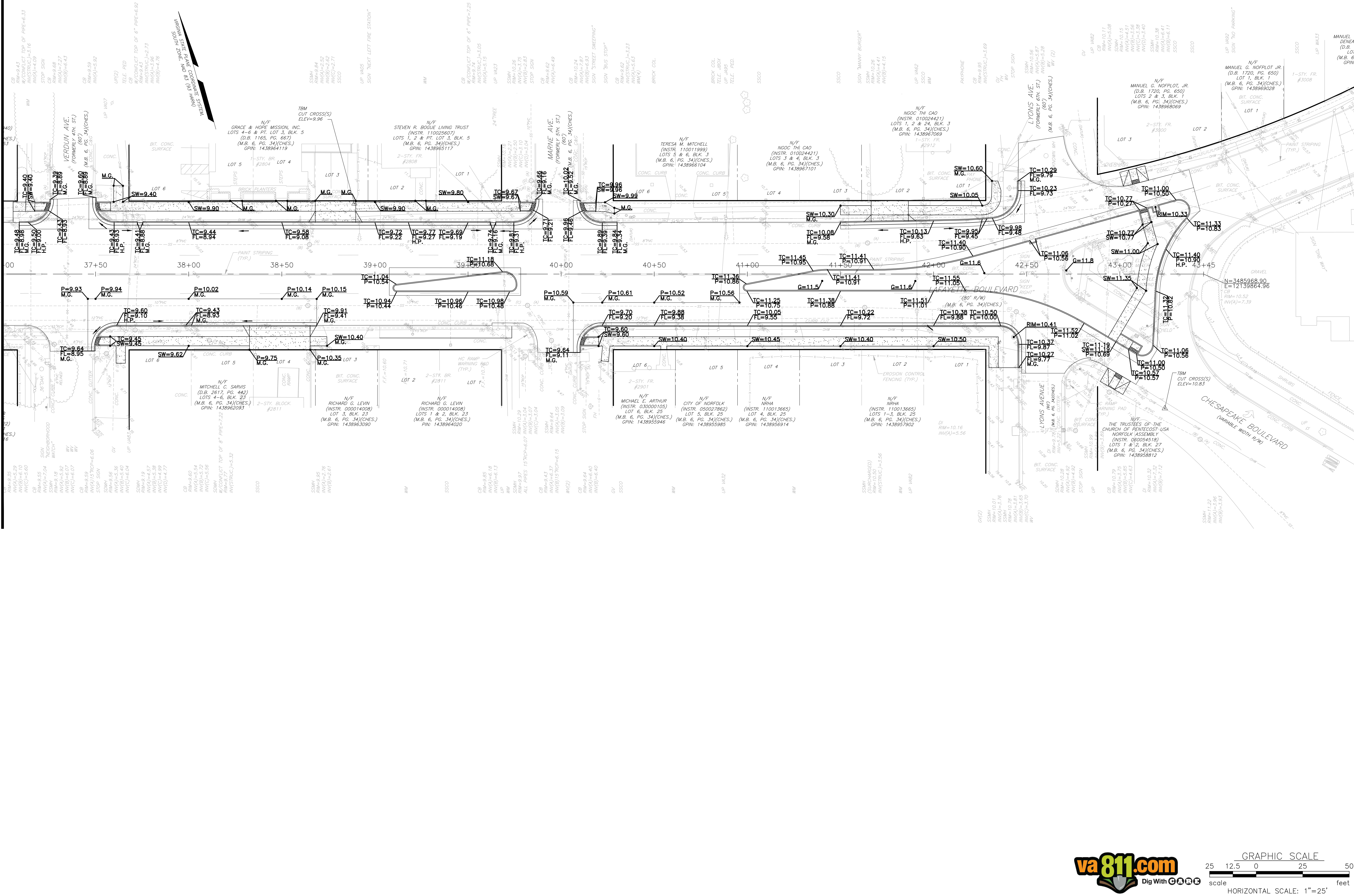


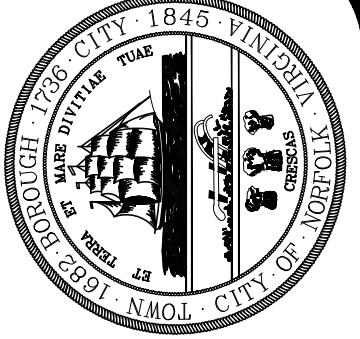
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CG-7.0

MATCHLINE- STATION 37+00, SEE SHEET CG-7.3 FOR CONTINUATION





6/03/16

STEPHEN M. BLEVINS
 No. 32265
 PROFESSIONAL ENGINEER

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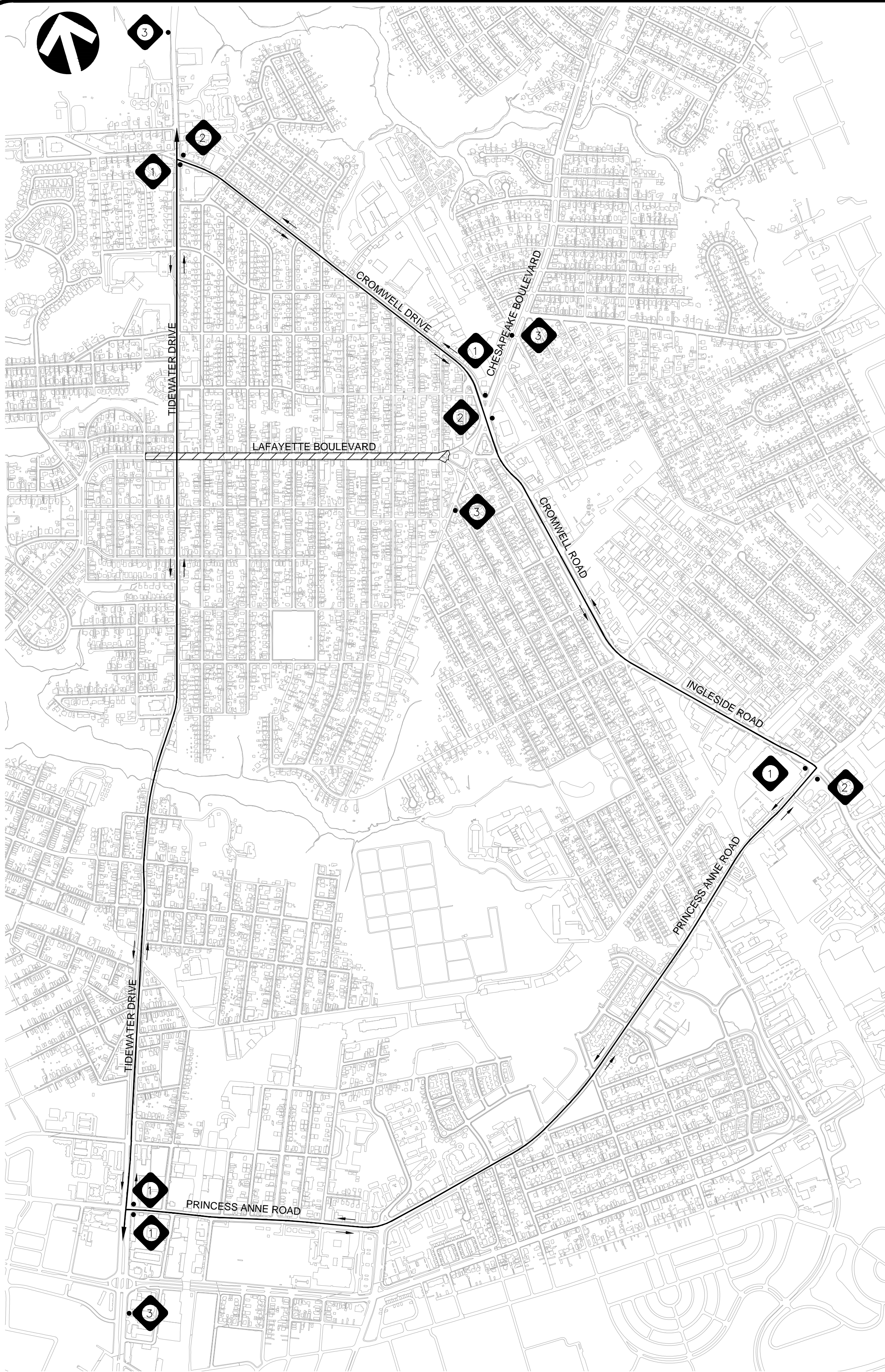
CITY OF NORFOLK
 DEPARTMENT OF PUBLIC WORKS

GRADING PLAN

LAFAYETTE BOULEVARD
 NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE PREPARED BY: E. TOLLE	5/24/16
2	ISSUE DATE:	
3	2/22/16	
4	SHEET	
5	CG-7.4	

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PHASE 1 - DETOUR ROUTING MAP
NOT TO SCALE

PHASE 1 - DETOUR ROUTING NOTES:

CONTRACTOR SHALL PROVIDE ALL DETOUR SIGNAGE NECESSARY ALONG ALL DETOUR ROUTES SHOWN. THE FOLLOWING DETOUR ROUTES SHALL BE ENFORCED DURING FULL ROAD CLOSURE OF LAFAYETTE BOULEVARD TO THROUGH TRAFFIC.

- TRAFFIC ON TIDEWATER DRIVE TRAVELING NORTH – TRAFFIC SHALL BE DETOURED TO CROMWELL DRIVE
- TRAFFIC ON TIDEWATER DRIVE TRAVELING SOUTH – TRAFFIC SHALL BE DETOURED TO PRINCESS ANNE ROAD
- TRAFFIC ON CHESAPEAKE BOULEVARD/CROMWELL DRIVE TRAVELING NORTH – TRAFFIC SHALL BE DETOURED TO CROMWELL DRIVE
- TRAFFIC ON CHESAPEAKE BOULEVARD TRAVELING SOUTH – TRAFFIC SHALL BE DETOURED TO PRINCESS ANNE ROAD

TEMPORARY TRAFFIC CONTROL NOTES:

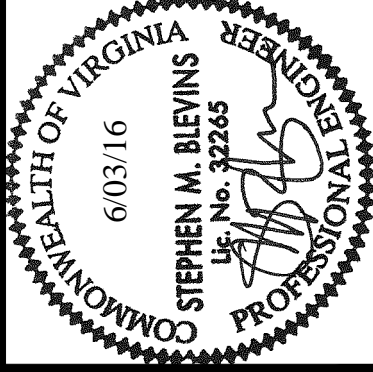
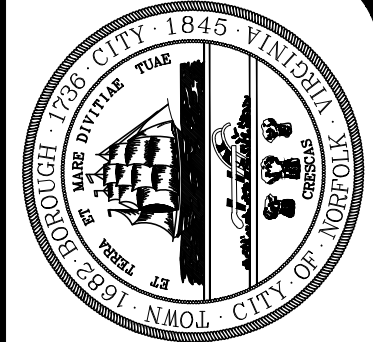
- THE CITY OF NORFOLK, TRANSPORTATION DEPARTMENT IS RESPONSIBLE FOR REVIEWING AND APPROVING ALL TRAFFIC MAINTENANCE AND CONTROL PLANS AND REVIEWING THE SEQUENCE OF CONSTRUCTION PLANS ESSENTIAL TO COMPLETE THIS PROJECT.
- ALL TRAFFIC MAINTENANCE AND CONTROL DEVICES, METHODS AND APPLICATIONS WILL CONFORM TO THE FOLLOWING PUBLICATIONS INCLUDING ALL CURRENT EDITIONS AND REVISIONS:
 - MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREET AND HIGHWAYS ISSUED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.(MUTCD)
 - VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE STANDARDS VOL. II.
 - VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE SPECIFICATIONS.
 - VIRGINIA DEPARTMENT OF TRANSPORTATION WORK AREA PROTECTION MANUAL.
 - CITY OF NORFOLK, PUBLIC WORKS SPECIFICATIONS AND STANDARDS MANUAL.
- WORK HOURS IN ROADWAY OPEN TO TRAFFIC AND/OR PEDESTRIAN AREA:
 - FOR WORK ON LAFAYETTE BOULEVARD FROM RACINE AVENUE TO CHESAPEAKE BOULEVARD, WORK HOURS SHALL BE MONDAY THROUGH SATURDAY, BETWEEN THE HOURS OF 9:00 A.M. AND 4:30 P.M.
 - FOR WORK ON LAFAYETTE BOULEVARD THAT WILL REQUIRE FULL ROAD CLOSURE, WORK HOURS SHALL BE SATURDAY AND SUNDAY ONLY, BETWEEN THE HOURS OF 9:00 A.M. AND 4:30 P.M.
 - HOLIDAYS: NO WORK MAY BE COMPLETED IN THE ROADWAY OR PEDESTRIAN AREAS UNLESS OTHERWISE NOTED OR APPROVED IN WRITING BY THE CITY.
 - ADDITIONAL RESTRICTIONS MAY APPLY BASED ON TRAFFIC CONDITIONS.

EXTENDED WORK HOURS AND WORK DAYS MAY BE REQUESTED WITH A WRITTEN REQUEST TO THE OWNER. THIS REQUEST MUST BE SUBMITTED AT LEAST FIVE (5) WORKING DAYS PRIOR TO THE EXTENDED WORK PERIODS. (PLEASE NOTE THAT IF WORK IS TO BE CONDUCTED IN THE RESORT AREA, CERTAIN RESTRICTIONS APPLY FROM APRIL TO OCTOBER. PLEASE CONTACT TRAFFIC ENGINEERING FOR THESE RESTRICTIONS).

- TRAFFIC CONTROL PLANS:TRAFFIC CONTROL PLANS ARE INCLUDED IN THE CONSTRUCTION PLANS FOR THE REFERENCED PROJECT; HOWEVER, FIELD CONDITIONS MAY REQUIRE MODIFICATIONS. IF THE CONTRACTOR FEELS THAT THE TRAFFIC CONTROL PLAN(S) INCLUDED WITH THIS PROJECT DOES NOT SUIT CONDITIONS AT A WORK SITE, THEN THE CONTRACTOR SHALL SUBMIT TO THE OWNER A REVISED PLAN TO MAINTAIN TRAFFIC. THE REVISED PLAN SHALL INCLUDE SITE SPECIFIC TRAFFIC DETAILS AND SHALL IDENTIFY THE SEQUENCE OF CONSTRUCTION. THE CONTRACTOR WILL SUBMIT THE REVISED TRAFFIC CONTROL PLAN(S) A MINIMUM 10 CALENDAR DAYS PRIOR TO THE START OF WORK. THE CONTRACTOR WILL NOT DISRUPT TRAFFIC PATTERNS UNTIL THE OWNER HAS APPROVED THE REVISED TRAFFIC CONTROL PLAN. THE OWNER RESERVES THE RIGHT TO MODIFY ANY TRAFFIC CONTROL PLAN(S) AS NECESSARY IN THE INTEREST OF PUBLIC SAFETY OR TRAFFIC EFFICIENCY. PRIOR TO BEGINNING WORK, IT IS THE CONTRACTORS RESPONSIBILITY TO INSURE THAT ALL REQUIREMENTS HAVE BEEN MET AND THAT ALL TRAFFIC CONTROL DEVICES HAVE BEEN INSTALLED ACCORDING TO THE APPROVED TRAFFIC CONTROL PLAN(S).
- THE CONTRACTOR SHALL CHECK ALL TRAFFIC MAINTENANCE AND CONTROL DEVICES AND WORK ZONES BEFORE, DURING, AND AFTER EACH WORK DAY TO ENSURE PROPER OPERATION. ON WEEKENDS, HOLIDAYS, OR ANY NON–WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING THE TRAFFIC MAINTENANCE AND CONTROL DEVICES DAILY FOR PROPER OPERATION.
- TWO–WAY TRAFFIC WILL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION UNLESS TRAFFIC ENGINEERING HAS APPROVED ANOTHER ALTERNATIVE FOR TRAFFIC CONTROL, SUCH AS A LANE CLOSURE OR A TEMPORARY STREET CLOSURE. RESIDENT AND EMERGENCY ACCESS WILL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION REGARDLESS IF A STREET CLOSURE IS IN EFFECT OR NOT.
- IF THERE IS AN APPROVAL FROM TRANSPORTATION DEPARTMENT FOR A LANE CLOSURE OR A TEMPORARY STREET CLOSURE, ALL LANES OF TRAFFIC WILL BE REOPENED TO TRAFFIC AT THE CONCLUSION OF EACH CONSTRUCTION DAY, UNLESS A 24–HOUR TEMPORARY STREET CLOSURE IS APPROVED AND IN EFFECT.
- IN ALL CASES IN WHICH EXISTING OR ESTABLISHED TRAFFIC PATTERNS WILL BE DISRUPTED, THE CONTRACTOR WILL NOTIFY ALL AFFECTED RESIDENTS AND/OR BUSINESSES A MINIMUM OF 48 HOURS IN ADVANCE OF THE ANTICIPATED DISRUPTION BY DISTRIBUTING DOOR–TO–DOOR NOTICES. A COPY OF THE NOTICE SHALL BE FORWARDED TO TRANSPORTATION DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO BEGINNING WORK.
- AT NIGHT OR DURING NON–CONSTRUCTION HOURS, ALL EXCAVATED AREAS ARE TO BE BACKFILLED OR SECURED AND PROTECTED BY USING APPROVED SAFETY DEVICES OR MATERIALS.
- WHEN THE USE OF TYPE III BARRICADES IS REQUIRED (DAY OR NIGHT), ALL BARRICADES WILL BE EQUIPPED WITH TYPE A HIGH–INTENSITY FLASHING WARNING LIGHTS.
- IN ACCORDANCE WITH THE VIRGINIA OCCUPATIONAL SAFETY AND HEALTH STANDARDS (CONSTRUCTION INDUSTRIAL) 29 CFR, PART 1929, 1989 EDITION, ALL CONTRACTOR EMPLOYEES AND SUBCONTRACTOR(S) EMPLOYEES EXPOSED TO VEHICULAR TRAFFIC WILL BE PROVIDED WITH AND REQUIRED TO WEAR WARNING VESTS MARKED WITH OR MADE OF REFLECTORIZED OR HIGH VISIBILITY MATERIALS.
- FOR CONSTRUCTION OPERATIONS LASTING MORE THAN 14 DAYS, THE CONTRACTOR WILL INSTALL A ROAD WORK AHEAD ® (W21–4) AND A END ROAD WORK ® (G20–2A) WARNING SIGNS ON 6” X 6” WOODEN GROUND MOUNTED POSTS. THESE SIGNS MUST BE INSTALLED PRIOR TO BEGINNING CONSTRUCTION WORK AND WILL BE REMOVED AFTER COMPLETION OF ALL CONSTRUCTION ACTIVITIES.
- FOR ALL APPROVED CONSTRUCTION/TRUCK ENTRANCES, THE CONTRACTOR WILL INSTALL “TRUCKS ENTERING HIGHWAY” (48” X 48” ORANGE AND BLACK) WARNING SIGNS ON 6” X 6” WOODEN GROUND MOUNTED POSTS. THESE SIGNS WILL BE INSTALLED 500 FEET IN ADVANCE OF ALL APPROVED CONSTRUCTION ACCESS/ENTRANCE POINTS.
- ANY TRAFFIC CONTROL DEVICES INCLUDING BUT NOT LIMITED TO PAVEMENT MARKINGS, SIGNS, AND TRAFFIC CONTROL SIGNAL EQUIPMENT DAMAGED OR DESTROYED BY THE CONTRACTOR MUST BE REPLACED AT THE CONTRACTOR’S EXPENSE UNLESS THEIR REMOVAL OR DESTRUCTION IS CALLED FOR BY THE PLANS.
- FOR ANY FURTHER INFORMATION ON TRAFFIC MAINTENANCE AND CONTROL REQUIREMENTS, PLEASE CONTACT THE TRANSPORTATION DEPARTMENT (757) 664–7300 FAX: (757) 664–7311.
- PROVIDE FLAGGERS DURING TEMPORARY CLOSURE OF THROUGH LANE WHILE TRUCKS ARE ENTERING AND PROJECT ACCESS.
- CONTRACTOR SHALL OBTAIN ALL APPLICABLE HAULING/RIGHT–OF–WAY PERMITS PRIOR TO CONSTRUCTION.

TEMPORARY TRAFFIC CONTROL LEGEND:

- | | |
|----------------------------|---------------------------------------|
| 1 M4–9R, DETOUR RIGHT | 7 R3–2, NO LEFT TURN |
| 2 M4–9L, DETOUR LEFT | 8 G20–2, END ROAD WORK |
| 3 W20–2, DETOUR AHEAD | 9 M4–8A, END DETOUR |
| 4 W20–3, ROAD CLOSED AHEAD | 10 TYPE 3 BARRICADE |
| 5 W20–1, ROAD WORK AHEAD | 11 R11–4, ROAD CLOSED TO THRU TRAFFIC |
| 6 R3–1, NO RIGHT TURN | WORK AREA |



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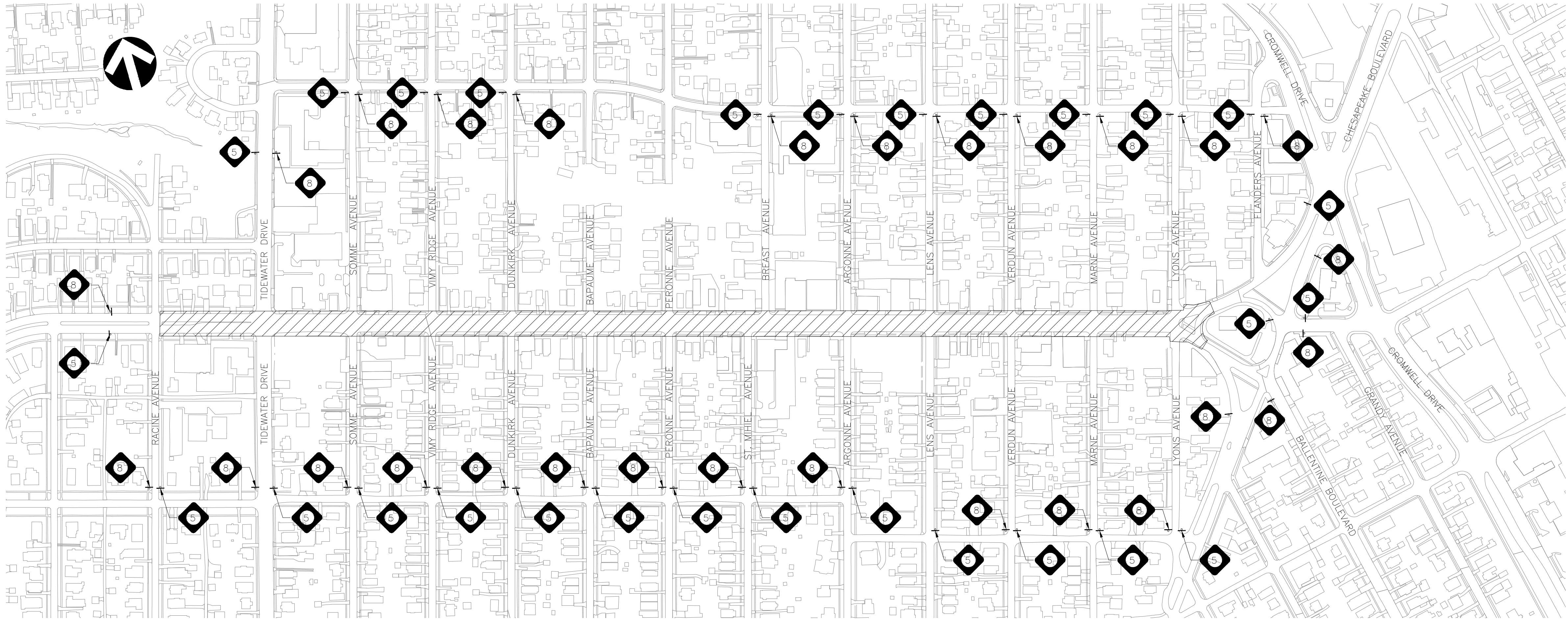
CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
MAINTENANCE OF TRAFFIC PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	REVISED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

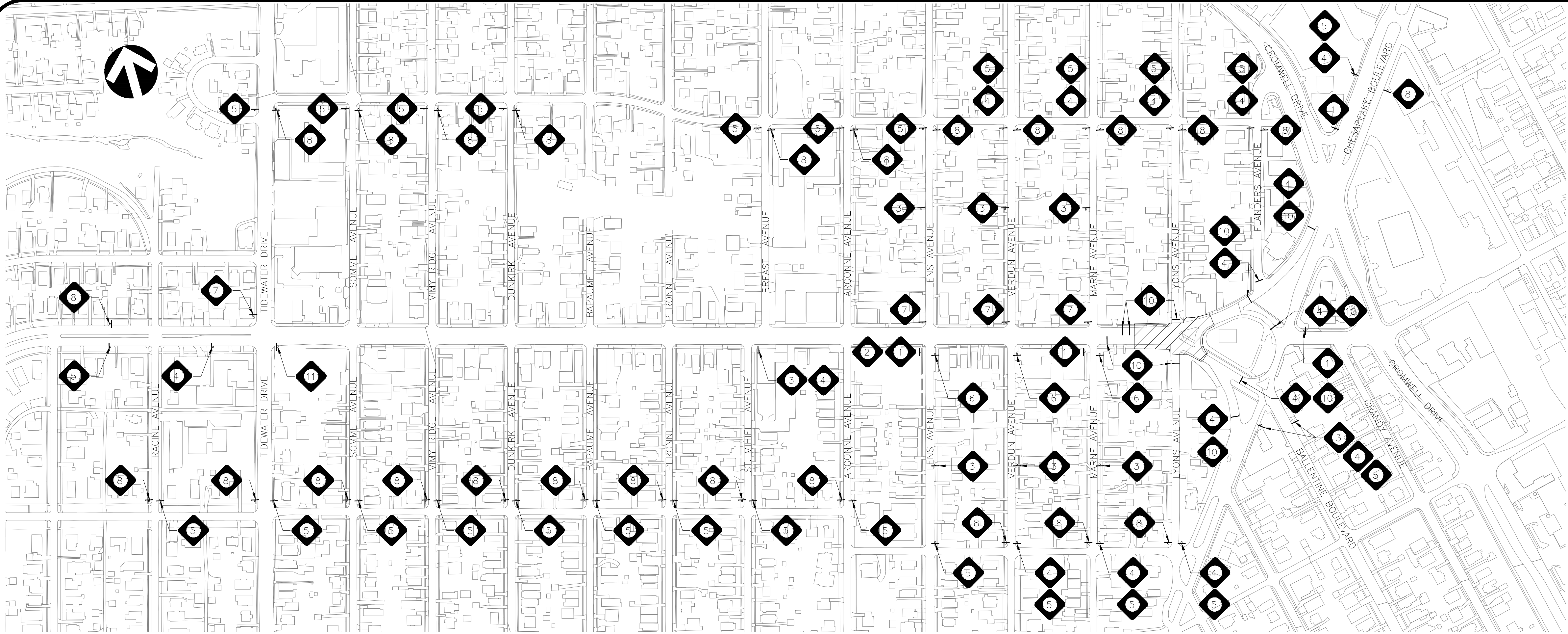
ISSUE DATE:
2/22/16

SHEET
CT-8.0



PHASE 2 - TEMPORARY TRAFFIC CONTROL

NOT TO SCALE

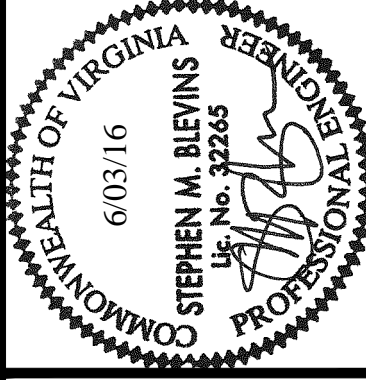
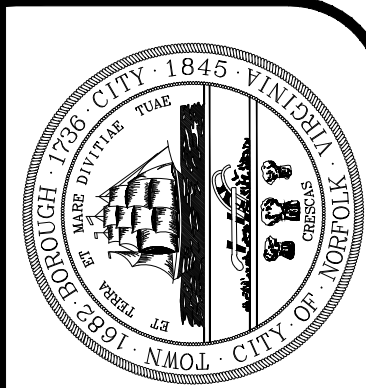


PHASE 1 - TEMPORARY TRAFFIC CONTROL

NOT TO SCALE

TEMPORARY TRAFFIC CONTROL LEGEND:

- 1 M4-9R, DETOUR RIGHT
 - 2 M4-9L, DETOUR LEFT
 - 3 W20-2, DETOUR AHEAD
 - 4 W20-3, ROAD CLOSED AHEAD
 - 5 W20-1, ROAD WORK AHEAD
 - 6 R3-1, NO RIGHT TURN
 - 7 R3-2, NO LEFT TURN
 - 8 G20-2, END ROAD WORK
 - 9 M4-8A, END DETOUR
 - 10 TYPE 3 BARRICADE
 - 11 R11-4, ROAD CLOSED TO THRU TRAFFIC
- WORK AREA

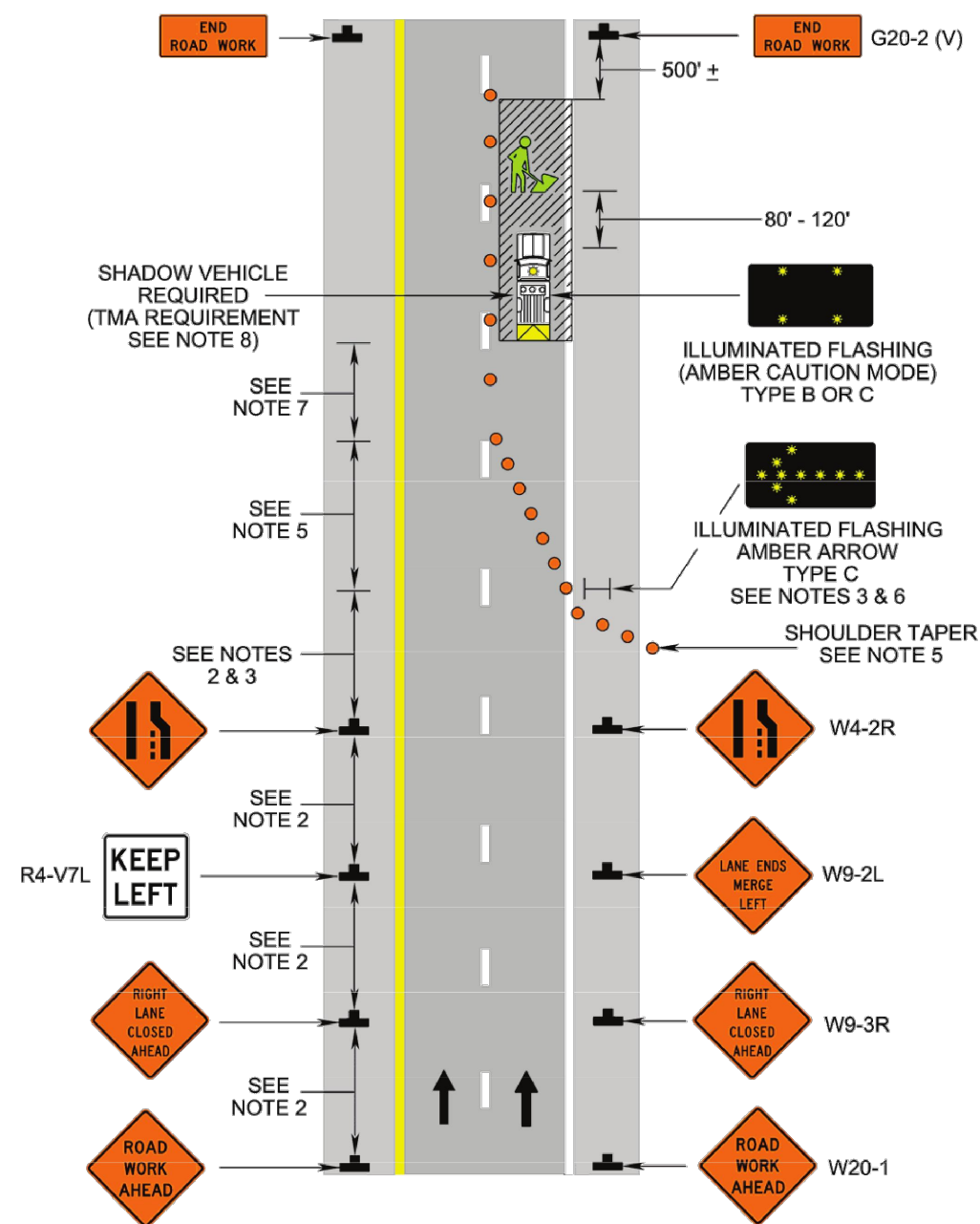


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CITY OF NORFOLK
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MAINTENANCE OF TRAFFIC PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE PREPARED BY: E. TOLLE	5/24/16
2	ISSUE DATE: 2/22/16	
3	SHEET CT-8.1	

Outside Lane Closure Operation on a Four-Lane Roadway
(Figure TTC-16.1)



Typical Traffic Control
Lane Closure Operation with Lane Weave
(Figure TTC-19.1)

NOTES

- Standard:**
- On divided highways having a median wider than 8', right and left sign assemblies shall be required.
- Guidance:**
- Sign spacing should be 1300'-1500' for Limited Access highways. For all other roadways, the sign spacing should be 500'-800' where the posted speed limit is greater than 45 mph, and 350'-500' where the posted speed limit is 45 mph or less.
 - Care should be exercised when establishing the limits of the work zone to insure maximum possible sight distance in advance of the transition, based on the posted speed limit and at least equal to or greater than the values in Table 6H-3. For Limited Access highways a minimum of 1000' is desired.
 - All vehicles, equipment, workers, and their activities should be restricted to one side of the pavement.

Standard:

- Taper length (L) and channelizing device spacing shall be:

Taper Length (L)					Channelizing Device Spacing		
Speed Limit (mph)	Lane Width (Feet)				Location	Speed Limit (mph)	
	9	10	11	12		0 - 35	36 +
25	95	105	115	125	Transition Spacing	20'	40'
30	135	150	165	180	Travelway Spacing	40'	80'
35	185	205	225	245	Construction Access*	80'	120'
40	240	270	295	320			
45	405	450	495	540			
50	450	500	550	600			
55	495	550	605	660			
60	540	600	660	720			
65	585	650	715	780			
70	630	700	770	840			

* Spacing may be increased to this distance, but shall not exceed one access per ¼ mile.

On roadways with paved shoulders having a width of 8 feet or more, channelizing devices shall be used to close the shoulder in advance of the merging taper to direct vehicular traffic to remain within the traveled way.

Minimum taper lengths for Limited Access highways shall be 1000 feet.

Shoulder Taper = ½ L Minimum

- An arrow board shall be used when a lane is closed. When more than one lane is closed, a separate arrow board shall be used lane.
- The buffer space length shall be shown in Table 6H-3 on Page 6H-5 for the posted speed limit.
- A shadow vehicle with either a Type B or C arrow board operating in the caution mode, or at least one high intensity amber rotating, flashing, or oscillating light shall be parked 80'-120' in advance of the first work crew. When the posted speed limit is 45 mph or greater, a truck-mounted attenuator shall be used.
- Vehicle hazard warning signals shall not be used instead of the vehicle's high-intensity amber rotating, flashing, or oscillating lights. Vehicle hazard warning signals can be used to supplement high-intensity amber rotating, flashing, or oscillating lights.
- When a side road intersects the highway within the TTC zone, additional TTC devices shall be placed as needed.

1: Revision 1 - 4/1/2015

Typical Traffic Control
Interior Lane Closure Operation on a Multi-Lane Roadway
(Figure TTC-42.1)

NOTES

Guidance:

- Sign spacing distance should be 1300'-1500' for Limited Access highways, and on all other roadways 500'-800' where the posted speed limit is greater than 45 mph, and 350'-500' where the posted speed limit is 45 mph or less.
- Care should be exercised when establishing the limits of the work zone to insure maximum possible sight distance in advance of the transition, based on the posted speed limit and at least equal to or greater than the values in Table 6H-3. For Limited Access highways a minimum of 1000' is desired.

Standard:

- On divided highways having a median wider than 8', right and left sign assemblies shall be required.
- Taper Length (L) and Channelizing Device Spacing shall be:

Taper Length (L)					Channelizing Device Spacing	
Speed Limit (mph)	Lane Width (Feet)				Location	Posted Speed (mph)
	9	10	11	12		
25	95	105	115	125	Transition Spacing	20' 40'
30	135	150	165	180	Travelway Spacing	40' 80'
35	185	205	225	245	Construction Access*	80' 120'
40	240	270	295	320		
45	405	450	495	540		
50	450	500	550	600		
55	495	550	605	660		
60	540	600	660	720		
65	585	650	715	780		
70	630	700	770	840		
Minimum taper lengths for Limited Access Highways shall be 1000 feet Shoulder Taper = 1/2 L Minimum						

* Spacing may be increased to this distance, but shall not exceed one access per 1/2 mile.

On roadways with paved shoulders having a width of 8 feet or more, channelizing devices shall be used to close the shoulder in advance of the merging taper to direct vehicular traffic to remain within the traveled way.

- The buffer space length shall be as shown in Table 6H-3 on Page 6H-5 for the posted speed limit.
- A shadow vehicle with either a Type B or C arrow board operating in the caution mode, or at least one high intensity amber rotating, oscillating, or flashing light shall be parked 80'-120' in advance of the first work crew. When the posted speed limit is 45 mph or greater, a truck-mounted attenuator shall be used.
- For long-term work zones existing conflicting pavement markings and markers shall be removed and temporary pavement markings and markers shall be installed per Figure TTC-60.

Option:

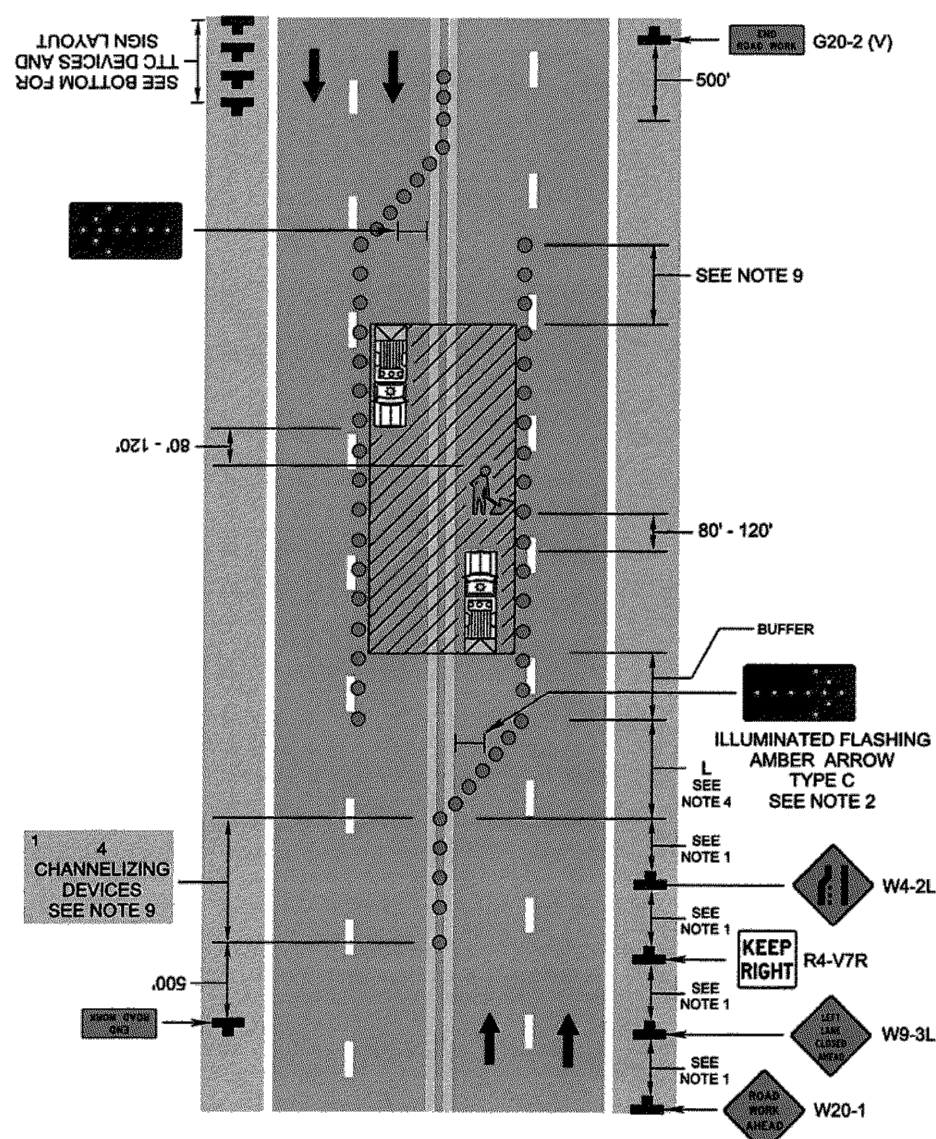
- For short-term stationary work (less than 3 days duration), lanes may be delineated by channelizing devices or removable pavement markings instead of temporary pavement markings.

Guidance:

- When channelizing devices have the potential of leading vehicular traffic out of the intended traffic space, the channelizing devices should be extended a distance with additional channelizing devices beyond the downstream end of the transition area as depicted.
- For locations with a high volume of left turning movements, the graphic NO LEFT TURN (R3-2) sign should be used.

1: Revision 1 - 4/1/2015

Interior Lane Closure Operation on a Multi-Lane Roadway
(Figure TTC-42.1)



1: Revision 1 - 4/1/2015

Typical Traffic Control
Street Closure Operation with Detour
(Figure TTC-34.1)

NOTES

Guidance:

- This plan should be used for streets without posted route numbers.
- On multi-lane streets, Detour signs with an Advance Turn Arrow should be used in advance of a turn.
- Sign spacing distance should be 225'-275' where the posted speed limit is 30 to 35 mph, and 100'-200' where the posted speed is 25 mph or less.
- If the road is opened for a significant distance beyond the intersection and/or there are significant origin/destination points beyond the intersection, the ROAD CLOSED (R11-2) and Detour Arrow (M4-10) signs on Type 3 Barricades should be located at the corners of intersecting closed roadway or the traveled way.

Option:

- Flashing warning lights and/or flags may be used to call attention to the advance warning signs.
- Flashing warning lights may be used on Type 3 Barricades.
- Detour signs may be located on the far side of intersections. A Detour sign with an advance arrow may be used in advance of a turn.
- A Street Name (M4-VP1a) plaque may be mounted with the Detour sign. The Street Name plaque may be either white on green or black on orange.

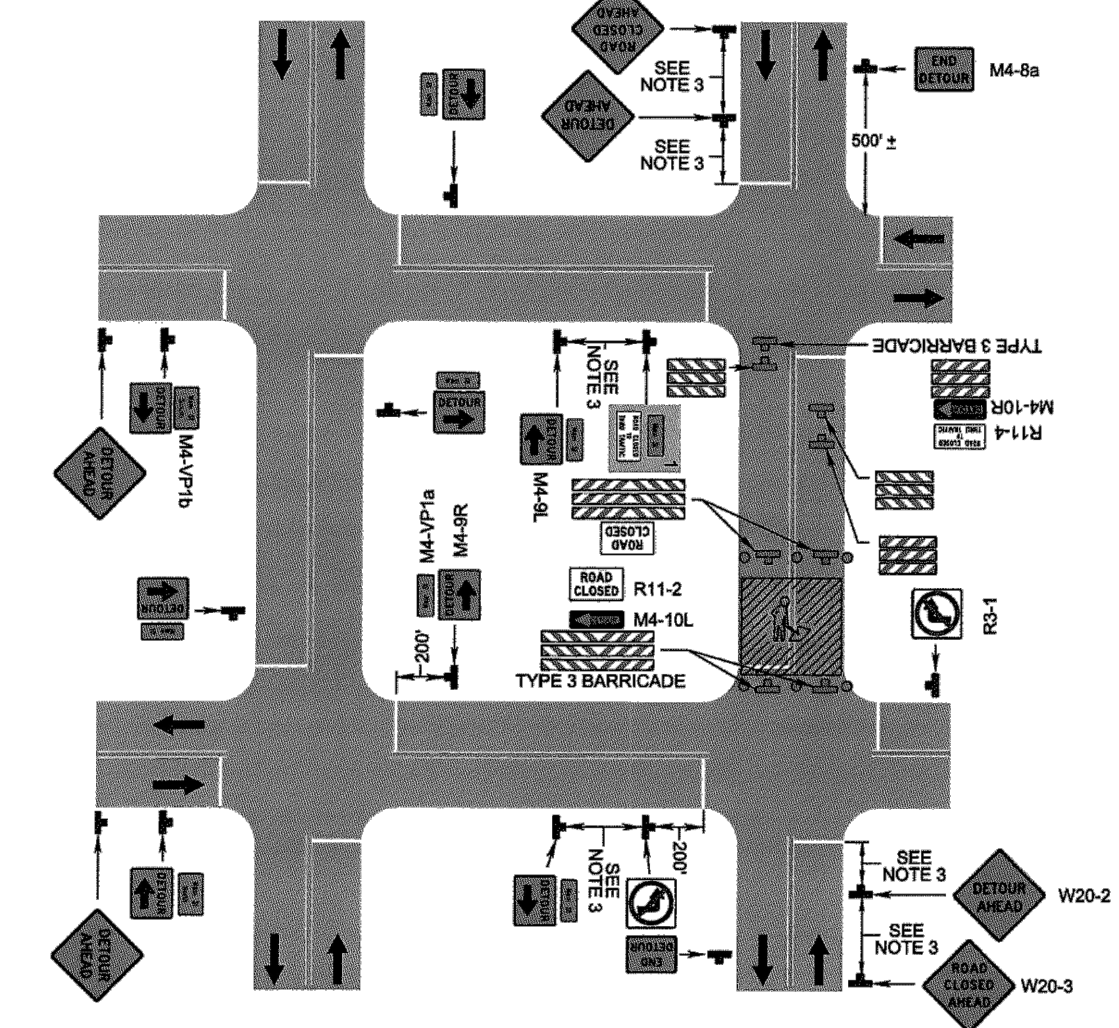
Standard:

- When used, the Street Name plaque shall be placed above the Detour sign.

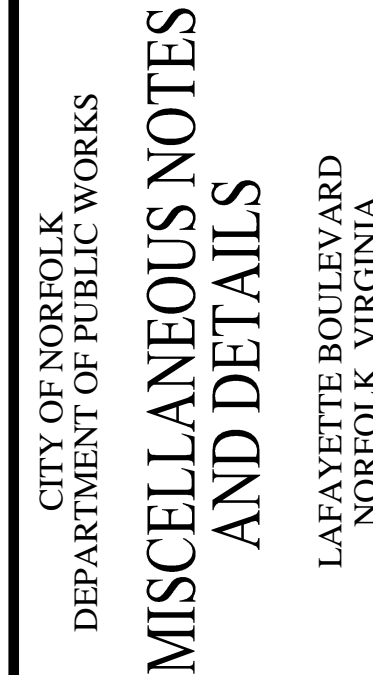
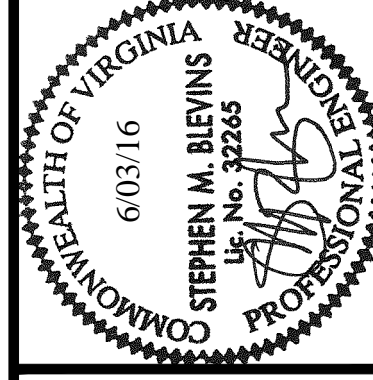
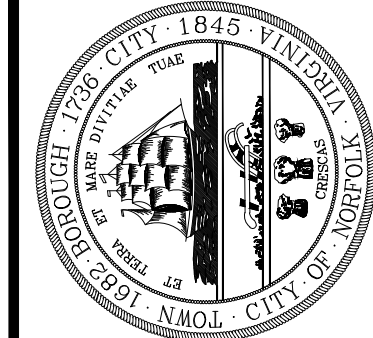
Support:

- See Chapter 61 for additional information on incident management traffic control.

Street Closure Operation with Detour
(Figure TTC-34.1)



1: Revision 1 - 4/1/2015



REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE PREPARED BY: E. TOLLE	5/24/16

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2/22/16
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CT-8.2

Typical Traffic Control
Shoulder Operation with Minor Encroachment
(Figure TTC-5.1)

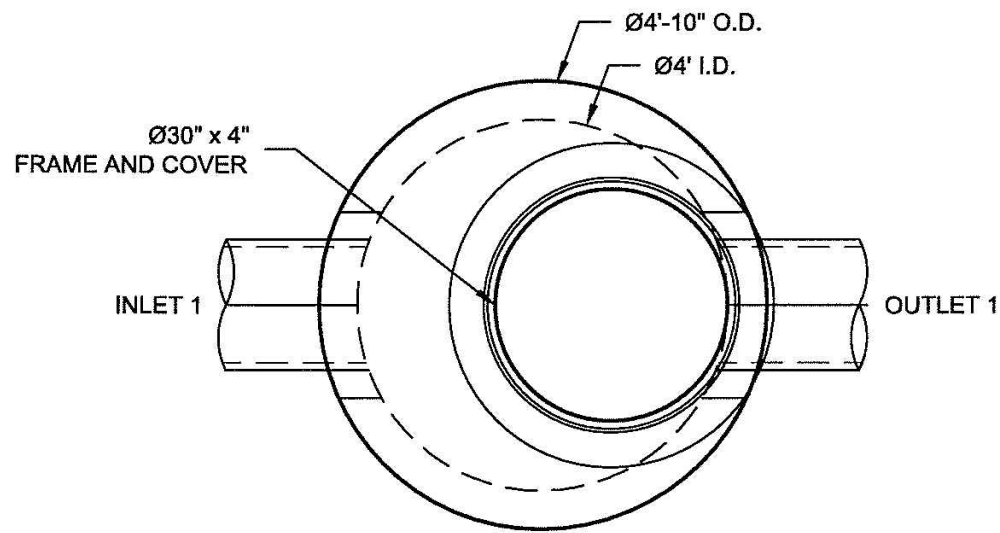
NOTES

- Standard
- For required sign assemblies for multi-lane roadways see Note 1, TTC-4.
- Guidance
- Sign spacing should be 1300'-1500' for Limited Access highways. For all other roadways, the sign spacing should be 500'-800' where the posted speed limit is greater than 45 mph, and 350'-500' where the posted speed limit is 45 mph or less.
 - When work takes up part of a lane on a high volume roadway, vehicular traffic volumes, vehicle mix, speed and capacity should be analyzed to determine whether the affected lane should be closed. Unless the lane encroachment analysis permits a remaining lane width of 10 feet, the lane should be closed. If the closure operation is on a Limited Access highway, the minimum lane width is 11 feet.
- Option:
- The ROAD WORK AHEAD (W20-1) sign on an intersecting roadway may be omitted where drivers emerging from that roadway will encounter another advance warning sign prior to this activity area.
- Standard:
- A shadow vehicle with either an arrow board operating in the caution mode, or at least one high-intensity amber rotating, flashing, or oscillating light shall be parked 80' - 120' in advance of the first work crew.
 - Vehicle hazard warning signals shall not be used instead of the vehicle's high-intensity amber rotating, flashing, or oscillating lights. Vehicle hazard warning signals can be used to supplement high-intensity amber rotating, flashing, or oscillating lights.
 - Taper length (L) and channelizing device spacing shall be at the following:
- | Taper Length (L) | | | | |
|---|-----|-----|-----|-----|
| Speed Limit (mph) | 9 | 10 | 11 | 12 |
| 25 | 95 | 105 | 115 | 125 |
| 30 | 135 | 150 | 165 | 180 |
| 35 | 185 | 205 | 225 | 245 |
| 40 | 240 | 270 | 295 | 320 |
| 45 | 405 | 450 | 495 | 540 |
| 50 | 450 | 500 | 550 | 600 |
| 55 | 495 | 550 | 605 | 660 |
| 60 | 540 | 600 | 660 | 720 |
| 65 | 585 | 650 | 715 | 780 |
| 70 | 630 | 700 | 770 | 840 |
| Minimum taper lengths for Limited Access highways shall be 1000 feet. | | | | |
| Shoulder Taper = 1/2 L Minimum | | | | |
- The buffer space length shall be as shown in Table 6H-3 on Page 6H-5 for the posted speed limit.
 - A truck-mounted attenuator (TMA) shall be used on Limited Access highways and multi-lane roadways with posted speed limit equal to or greater than 45 mph.
 - When a side road intersects the highway within the temporary traffic control zone, additional traffic control devices shall be placed as needed.
- 1: Revision 1 - 4/1/2015

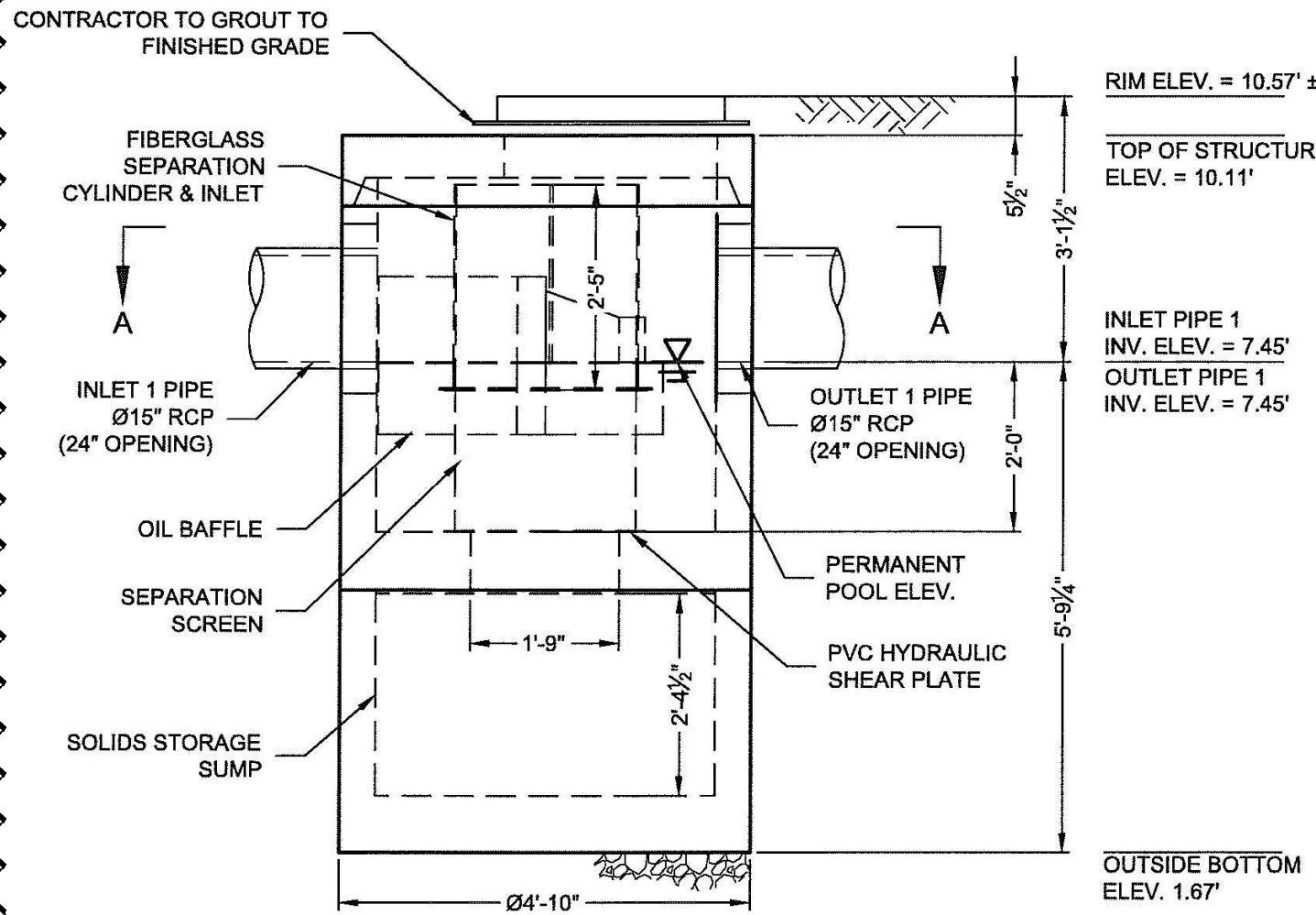
Typical Traffic Control
Sidewalk Closure and Bypass Sidewalk Operation
(Figure TTC-35.0)

NOTES

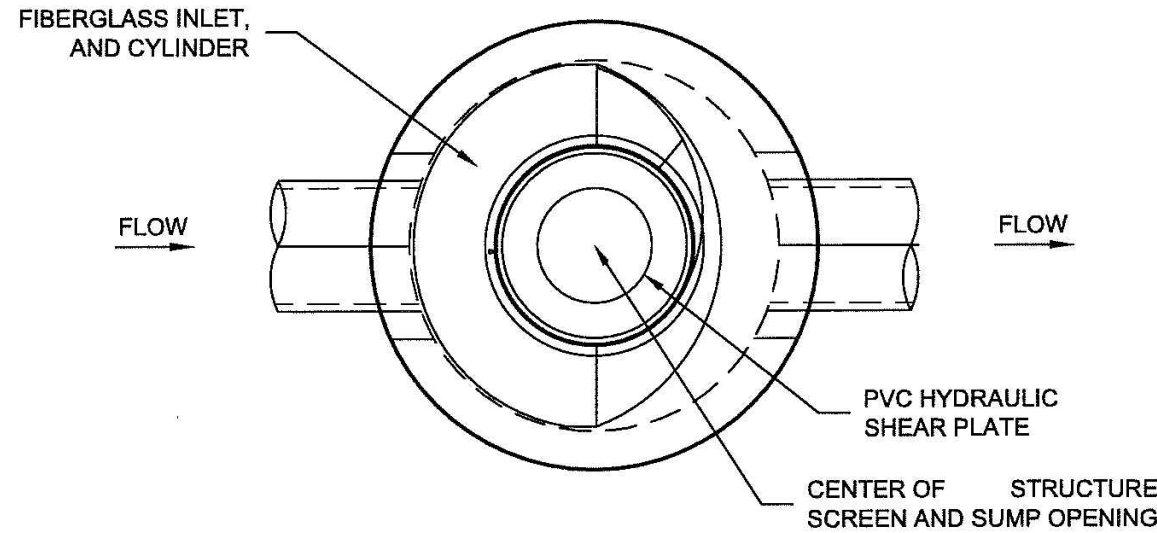
- Standard:
- When crosswalks or other pedestrian facilities are closed or relocated, temporary facilities shall be detectable and shall include accessibility features consistent with the features present in the existing pedestrian facility.
- Guidance:
- Where high speeds are anticipated, a temporary traffic barrier and, if necessary, a crash cushion should be used to separate the temporary sidewalks from vehicular traffic.
 - Audible information devices should be considered where midblock closings and changed crosswalk areas cause inadequate communication to be provided to pedestrians who have visual disabilities.
 - Temporary markings should be considered for operations exceeding three days in duration.
- Option:
- Only the TTC devices related to pedestrians are shown. Other devices, such as lane closure signing or ROAD NARROWS (W5-1) signs, may be used to control vehicular traffic.
 - For nighttime closures, Type A Flashing warning lights may be used on barricades that support signs and close sidewalks.
 - Signs, such as KEEP RIGHT (R4-V7R) and KEEP LEFT (R4-V7L), may be placed along a temporary sidewalk to guide or direct pedestrians.
- Standard:
- All sidewalk closures shall be closed with Type 3 Barricades.



PLAN VIEW



ELEVATION VIEW



SECTION A-A

COUNT	DESCRIPTION	INSTALLED BY
1	FIBERGLASS INLET AND CYLINDER	MANUFACTURER
1	2400 micron, 2' O.D. x 1.67' SEP. SCREEN	MANUFACTURER
1	3/16 INCH PVC HYDRAULIC SHEAR PLATE	MANUFACTURER
1	SEALANT FOR JOINTS (BY PRECASTER)	CONTRACTOR
1	Ø30" x 4' FRAME & COVER, EJ#41600484, OR EQUIV.	CONTRACTOR

SITE DESIGN DATA

WATER QUALITY FLOW RATE	0.31 CFS
PEAK FLOW RATE	1.37 CFS
RETURN PERIOD OF PEAK FLOW	10 YRS

GENERAL NOTES

- MANUFACTURER TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
 - FOR FABRICATION DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, REFER TO THE MANUFACTURER.
 - THE WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING. CONTRACTOR TO CONFIRM STRUCTURE MEETS REQUIREMENTS OF PROJECT.
 - STRUCTURE SHALL MEET AASHTO HS-20 LOAD RATING, ASSUMING EARTH COVER OF 0' - 2' AND GROUNDWATER ELEVATION AT, OR BELOW, THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION. CASTINGS SHALL MEET AASHTO M306.
 - IF REQUIRED, PVC HYDRAULIC SHEAR PLATE IS PLACED ON SHELF AT BOTTOM OF SCREEN CYLINDER. REMOVE AND REPLACE AS NECESSARY DURING MAINTENANCE CLEANING.
 - THE STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO ASTM C-478 AND AASHTO LOAD FACTOR DESIGN METHOD.
- INSTALLATION NOTES
- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
 - CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE MANHOLE STRUCTURE.
 - CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
 - CONTRACTOR TO PROVIDE, INSTALL, AND GROUT INLET AND OUTLET PIPE(S). MATCH PIPE INVERTS WITH ELEVATIONS SHOWN. ALL PIPE CENTERLINES TO MATCH PIPE OPENING CENTERLINES.
 - CONTRACTOR TO TAKE APPROPRIATE MEASURES TO ASSURE UNIT IS WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. IT IS SUGGESTED THAT ALL JOINTS BELOW PIPE INVERTS ARE GROUTED.

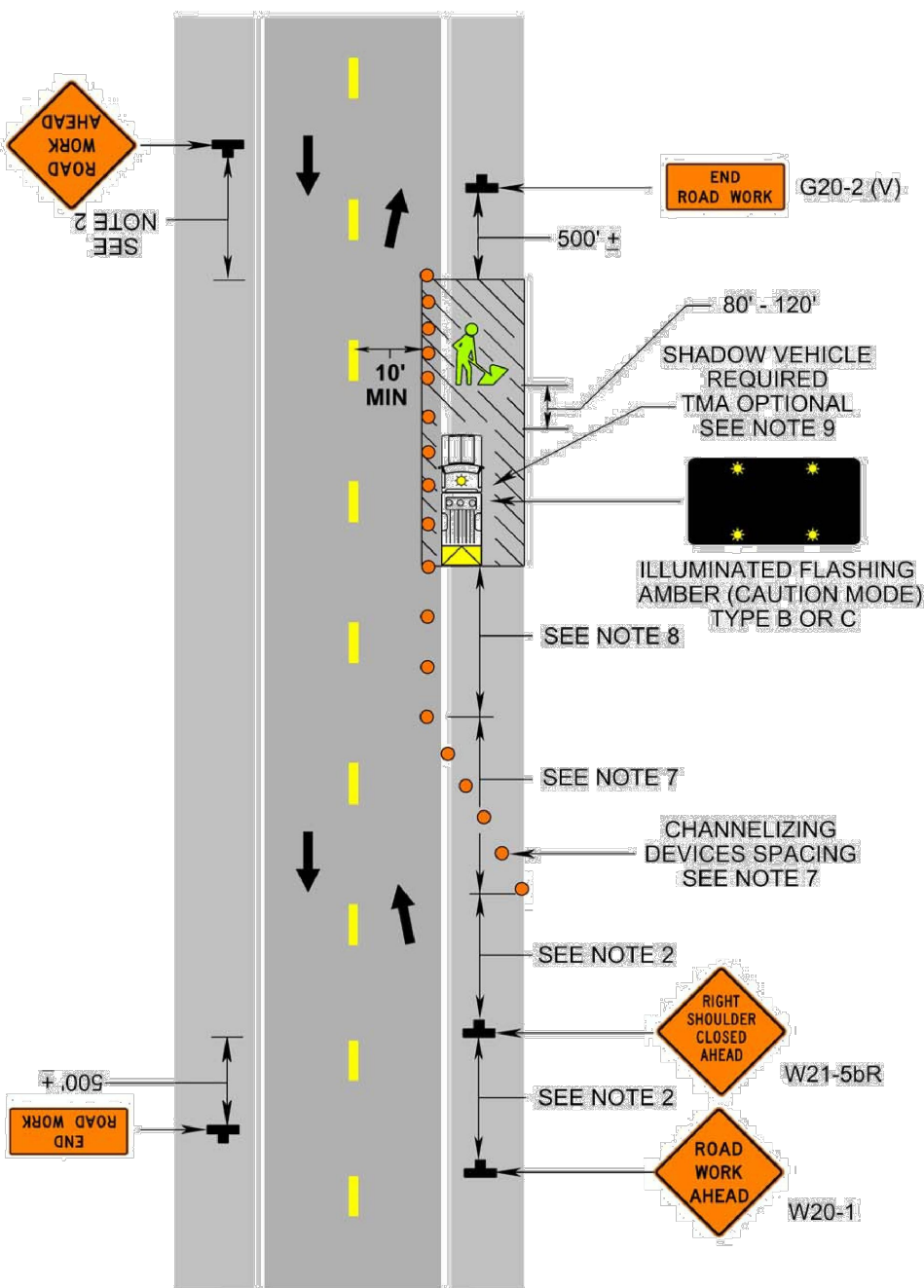
STRUCTURE WEIGHT
APPROXIMATE HEAVIEST PICK = 6500 LBS.
STRUCTURE IS DELIVERED IN 3 PIECES

MAX FOOTPRINT = Ø4'-10"

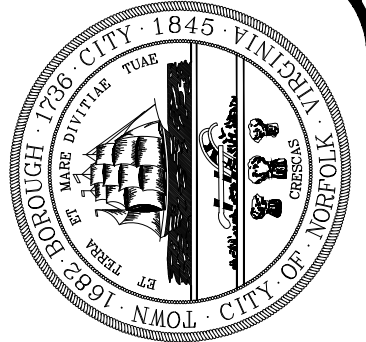
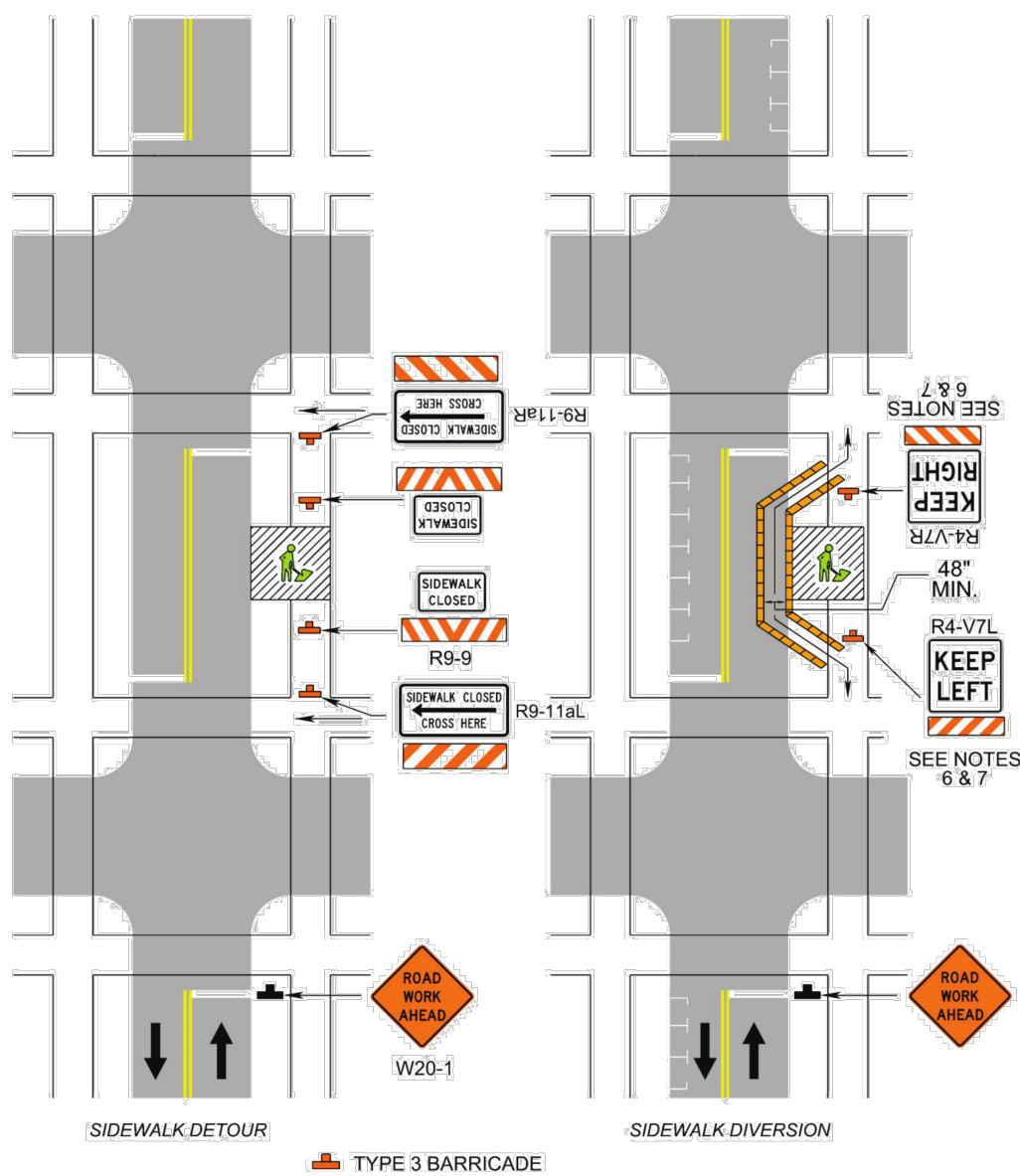
HYDRODYNAMIC SEPARATOR DETAIL

NOT TO SCALE

Shoulder Operation with Minor Encroachment
(Figure TTC-5.1)



Sidewalk Closure and Bypass Sidewalk Operation
(Figure TTC-35.0)

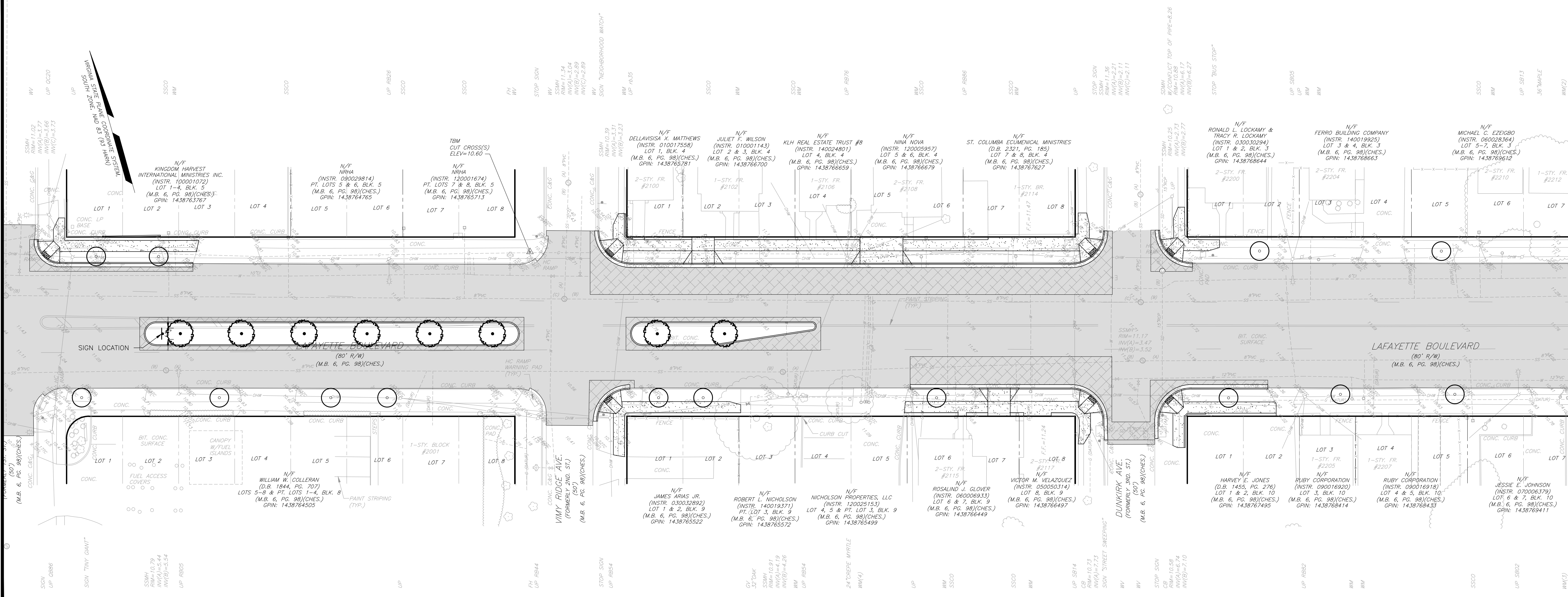


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1	DESIGNED BY: E. TOLLE PREPARED BY: E. TOLLE	5/24/16

ISSUE DATE:
2/22/16

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CT-8.3

NO PROPOSED LANDSCAPING FROM STATION 10+00 TO 16+50



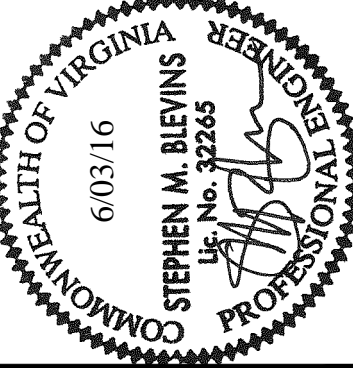
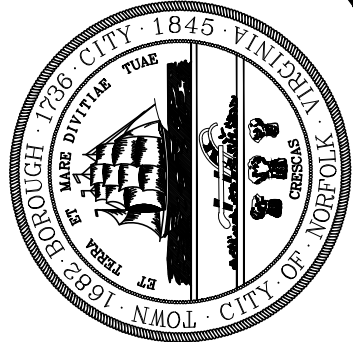
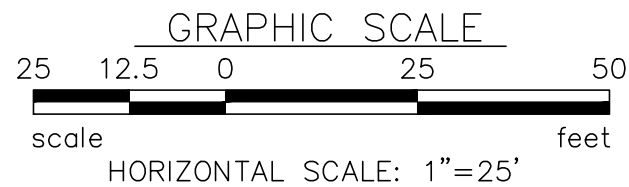
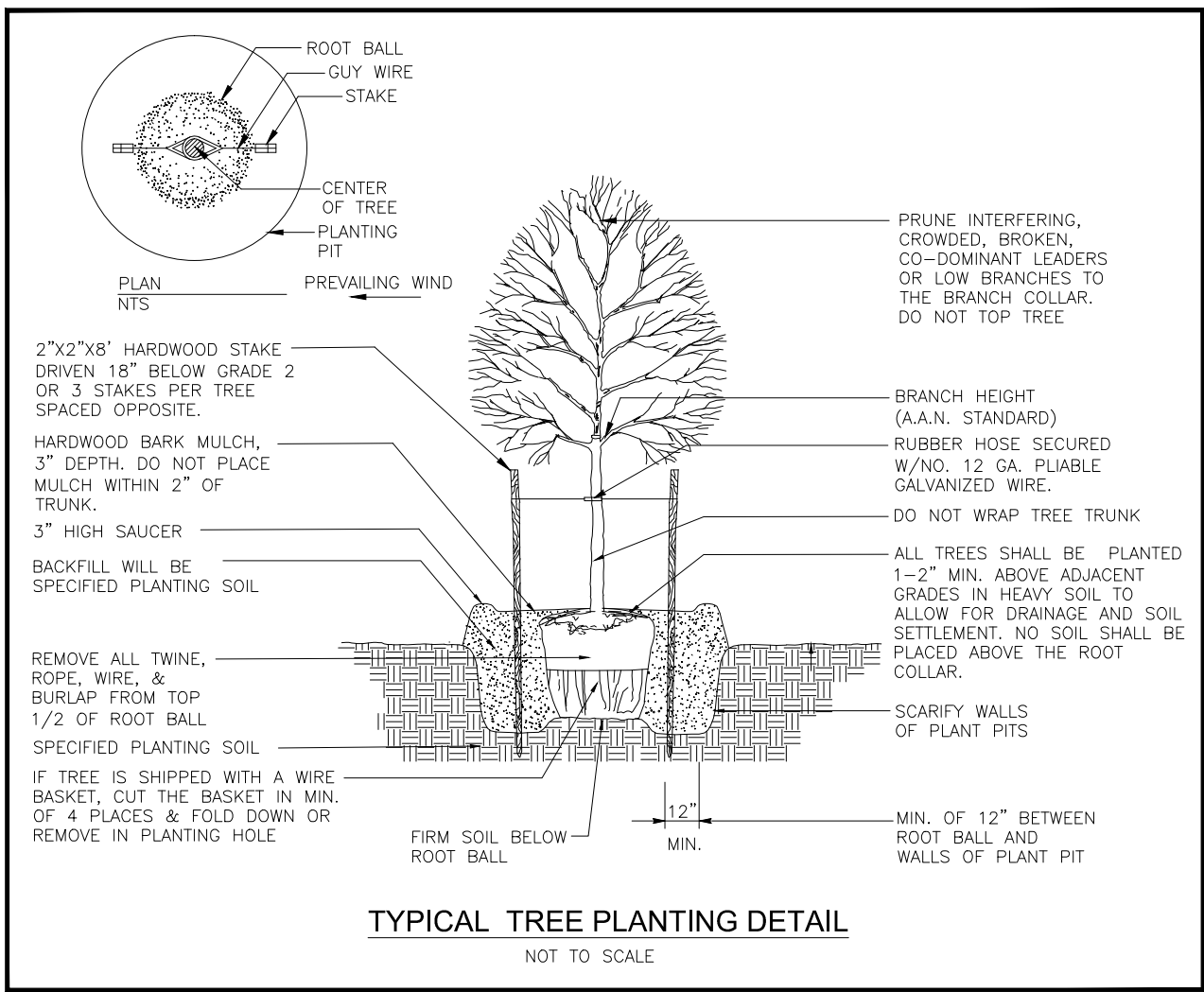
MATCHLINE- STATION 23+50, SEE SHEET LP-9.1 FOR CONTINUATION

PLANT KEY

BOTANICAL NAME	COMMON NAME	MINIMUM SIZE
QUERCUS MICHAUXII	(SWAMP CHESTNUT OAKS)	2" - 2-1/2" CAL. (8'-10' HT.)
LAGERSTROEMIA INDICA	(MUSKOGEE CRAPE MYRTLE)	2" - 2-1/2" CAL. (8'-10' HT.)

CITY OF NORFOLK LANDSCAPE NOTES

- All required trees shall be a minimum of 2 1/2 inches in caliper and eight feet in overall height at the time of installation and of varieties, which shall attain mature spreads of at least 20 feet.
- All landscaping materials shall meet the minimum specifications and standards described in the most current edition of "American Standard for Nursery Stock," 1986, published by the American Association of Nurserymen, 1280 I Street, N.W.: Suite 500, Washington, D.C. 20005.
- Planting season restrictions:
 - All balled and burlapped plants shall be installed between October 15 and March 31, unless otherwise authorized by the Department of Neighborhood and Leisure Services.
 - All container grown plants shall be installed between September 15 and May 15, unless otherwise authorized by the Department of Neighborhood and Leisure Services.
 - Any planting dates other than those noted above shall be considered out-of-season. At such times, a bond or other form of surety covering the cost of installation of all landscaping shall be required in conformance with provisions set forth in section 17-11.
- SOIL NOTE:** All soil in interior landscape islands shall be replaced with a minimum of 24" of topsoil. The source of the replacement soil shall be submitted to the Bureau of Parks and Forestry for approval. Do not remove and replace soil in the vicinity of existing trees to be retained.
- Contractor is required to obtain a Tree Permit from Parks and Urban Forestry for any tree removal or trimming on trees on City of Norfolk property or within the Right-of-Way.
- Tree removal or trimming must be done by a certified Arborist.

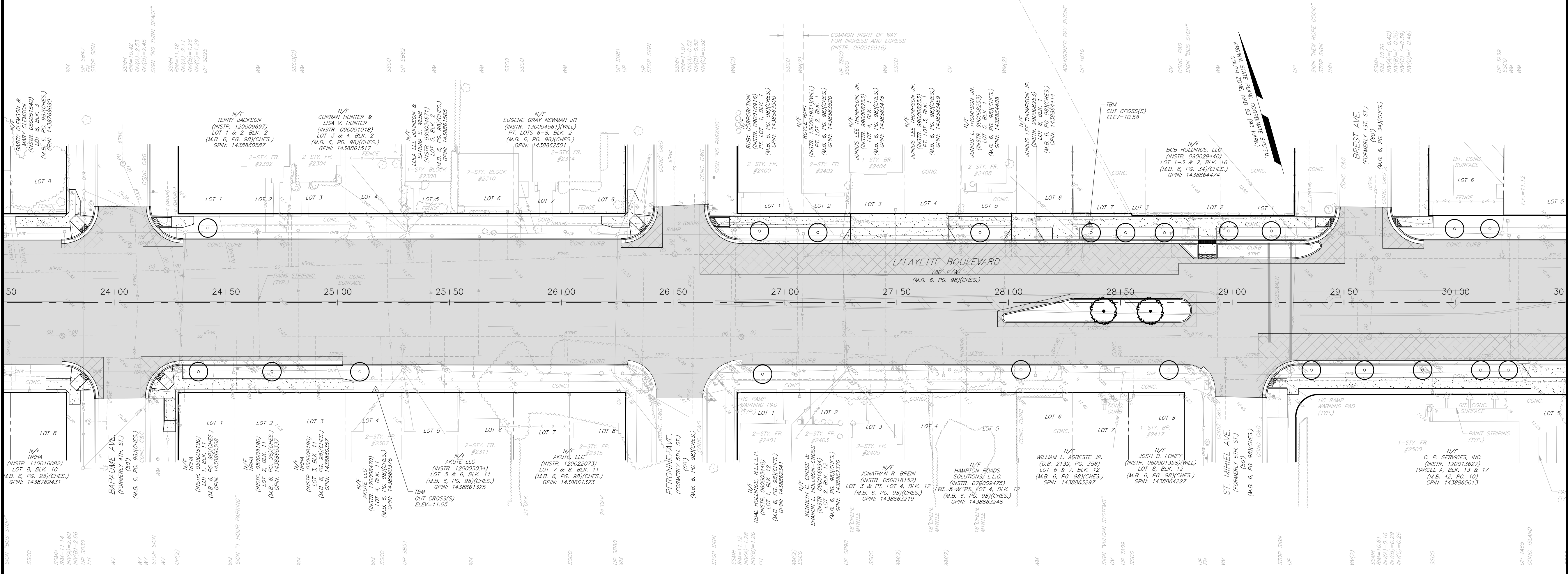


CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS
LANDSCAPE PLAN
LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA


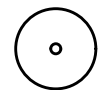
REV #	PROJECT REVISIONS	DATE
1	DESIGNED BY: E. TOLLE	5/24/16
2	PREPARED BY: E. TOLLE	
3	ISSUE DATE:	
4		
5		
6		
7		
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DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE
ISSUE DATE:
2/22/16
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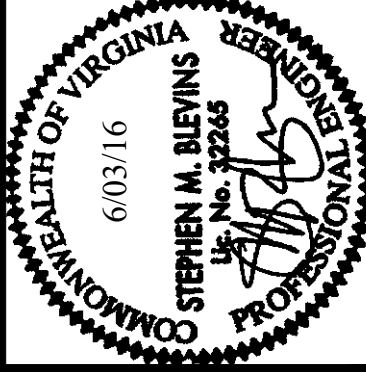
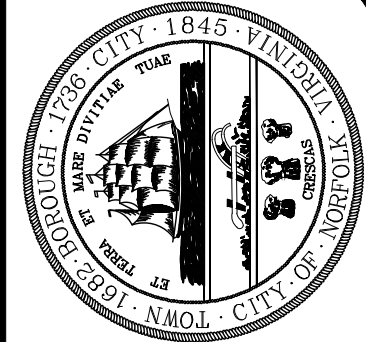
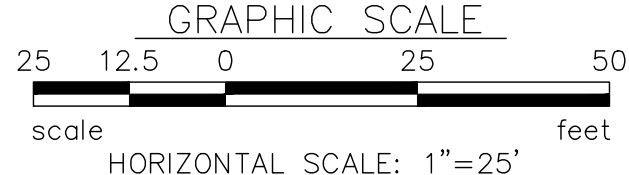
MATCHLINE- STATION 23+50, SEE SHEET LP-9.0 FOR CONTINUATION



PLANT KEY

BOTANICAL NAME	COMMON NAME	MINIMUM SIZE
	QUERCUS MICHAUXII (SWAMP CHESTNUT OAKS)	2" - 2-1/2" CAL. (8'-10' HT.)
	LAGERSTROEMIA INDICA (MUSKOGEE CRAPE MYRTLE)	2" - 2-1/2" CAL. (8'-10' HT.)

MATCHLINE- STATION 30+50, SEE SHEET LP-9.2 FOR CONTINUATION



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CITY OF NORFOLK
DEPARTMENT OF PUBLIC WORKS

LANDSCAPE PLAN

LAFAYETTE BOULEVARD
NORFOLK, VIRGINIA

REV. #	PROJECT REVISIONS	DATE
1	DESIGNED PER CITY COMMENTS DATED 3/3/16	5/24/16

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

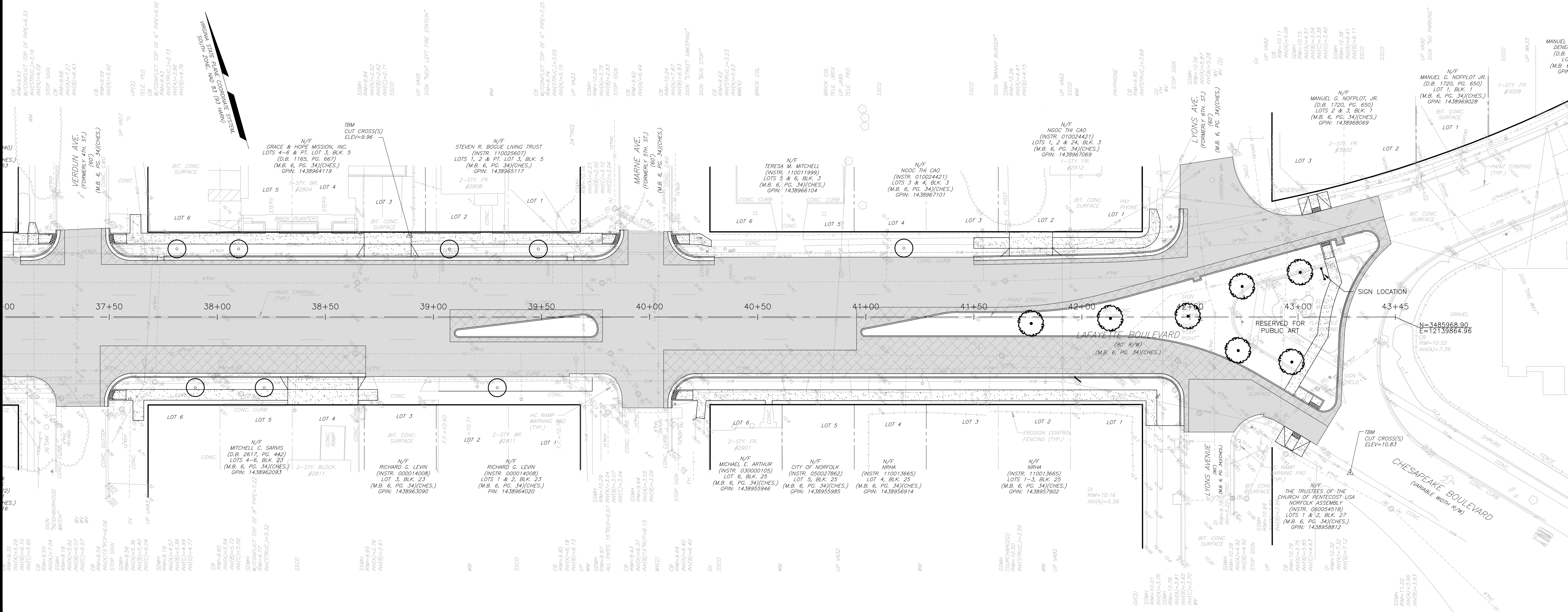
ISSUE DATE:
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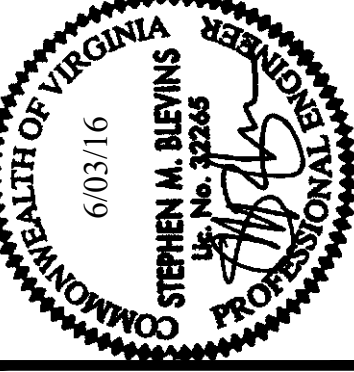
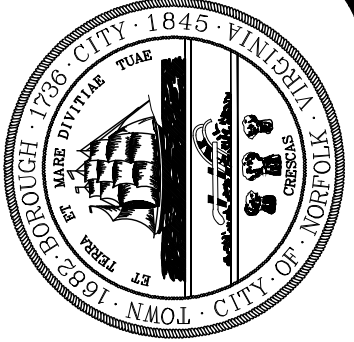
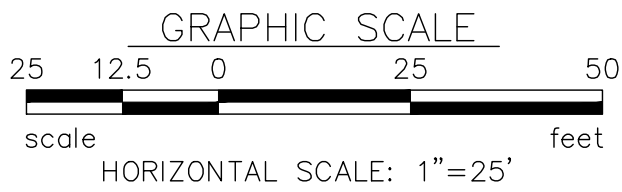
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MATCHLINE- STATION 37+00, SEE SHEET LP-9.2 FOR CONTINUATION



PLANT KEY

	BOTANICAL NAME	COMMON NAME	MINIMUM SIZE
	QUERCUS MICHAUXII	(SWAMP CHESTNUT OAKS)	2" - 2-1/2" CAL. (8'-10' HT.)
	LAGERSTROEMIA INDICA	(MUSKOGEE CRAPE MYRTLE)	2" - 2-1/2" CAL. (8'-10' HT.)



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LANDSCAPE PLAN

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NORFOLK, VIRGINIA

[illegible]

DESIGNED BY: E. TOLLE
PREPARED BY: E. TOLLE

ISSUE DATE:
2/22/16

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LP-9.3